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DESIGN DESIGNATION

W. BROADWAY ST.

OPENING YEAR A.D.T. (2023)	1,100
DESIGN A.D.T. (2043)	1,300
DESIGN HOURLY VOLUME (2043)	130
TRUCKS (24 HOUR)	1%
DESIGN SPEED	35 M.P.H.
LEGAL SPEED	35 M.P.H.
DESIGN FUNCTIONAL CLASSIFICATION~	URBAN MAJOR COLLECTOR
NHS PROJECT	NO
DESIGN EXCEPTIONS	NONE

UNDERGROUND UTILITIES

Contact Two Working Days
Before You Dig



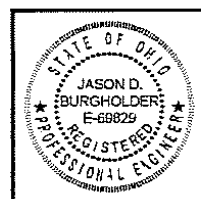
OHIO811, 8-1-1, or 1-800-362-2764
(Non-members must be called directly)

THIS PLAN PREPARED AND RECOMMENDED BY:

52 N. Diamond Street
Mansfield, OH 44902
T: 419-525-0093

526 E. Broad Street
Elyria, OH 44035
T: 440-323-9608

K.E. McCARTNEY & ASSOCIATES
ENGINEERS • PLANNERS • SURVEYORS

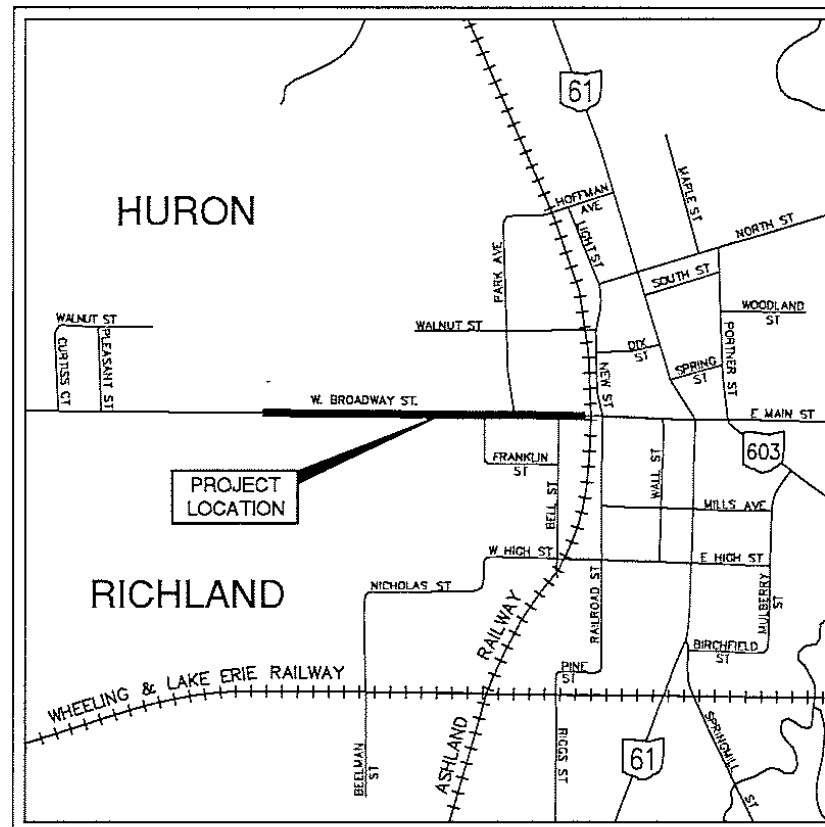


JASON D. BURGHOLDER

 1/18/2023

 DATE

VILLAGE OF PLYMOUTH W. BROADWAY STREET DRAINAGE IMPROVEMENTS HURON & RICHLAND COUNTIES STATE OF OHIO



LOCATION MAP

NOT TO SCALE

LATITUDE: 40° 59' 43" N LONGITUDE: 82° 40' 19" W

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	
CB-1	7/16/21	MT-95.31	7/19/19	800 - 2019, SEE PROPOSAL	
CB-3	7/16/21	MT-97.10	4/19/19	821 4/20/12	
CB-3A	7/16/21	MT-101.90	7/17/20	832 7/15/22	
CB-6	1/21/22	MT-110.10	7/19/13	902 7/19/19	
MH-3	7/16/21				
DM-1.1	7/17/20				
DM-4.4	1/15/16				
BP-4.1	7/19/13				
BP-5.1	7/15/22				
				SPECIAL PROVISIONS	

PROJECT DESCRIPTION

DRAINAGE IMPROVEMENT PROJECT FOR 2,030 FEET ALONG WEST BROADWAY STREET IN THE VILLAGE OF PLYMOUTH. THE PROJECT INCLUDES NEW CURB, STORM SEWER, CATCH BASINS, AND MANHOLES.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA.....	0.84	ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA.....	0.13	ACRES
NOTICE OF INTENT (NOI) EARTH DISTURBED AREA.....	N/A	ACRES
(NOI NOT REQUIRED)		

2019 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

TRAFFIC NOT RE-ROUTED

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVAL:

CASSAUNDR FRYMAN, MAYOR

 DATE

JAMES BURTON, VILLAGE ADMINISTRATOR

 DATE

PID. NO.
N/A

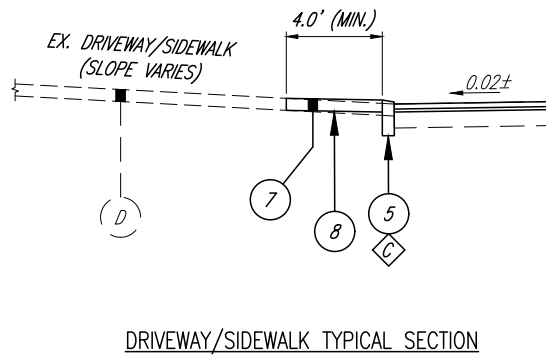
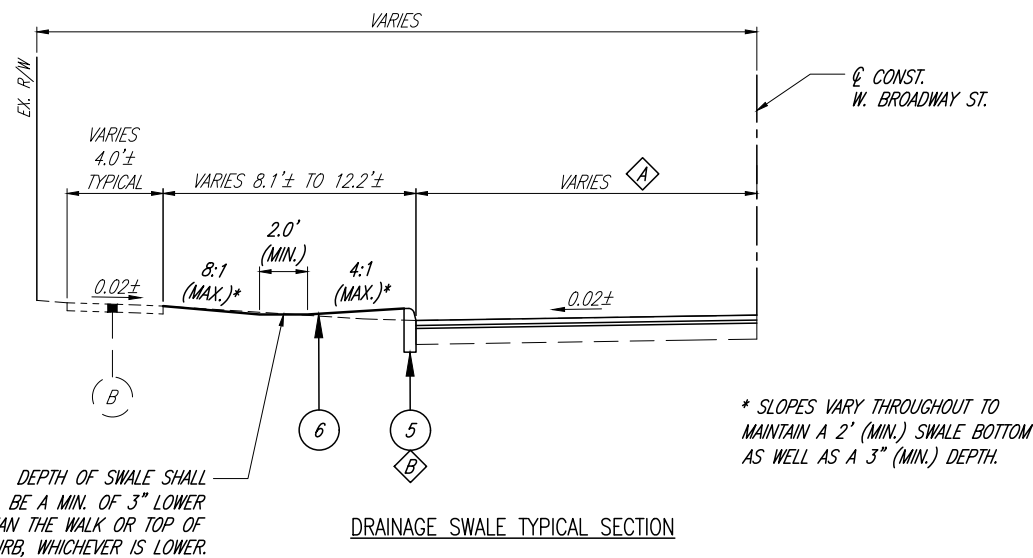
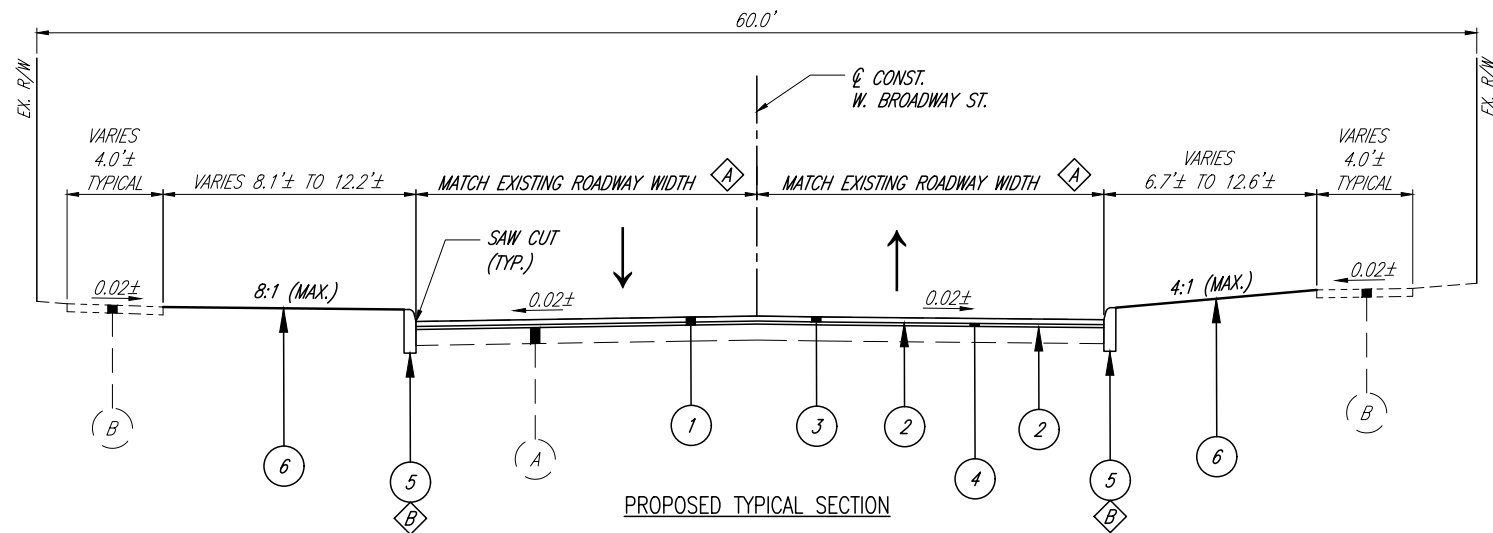
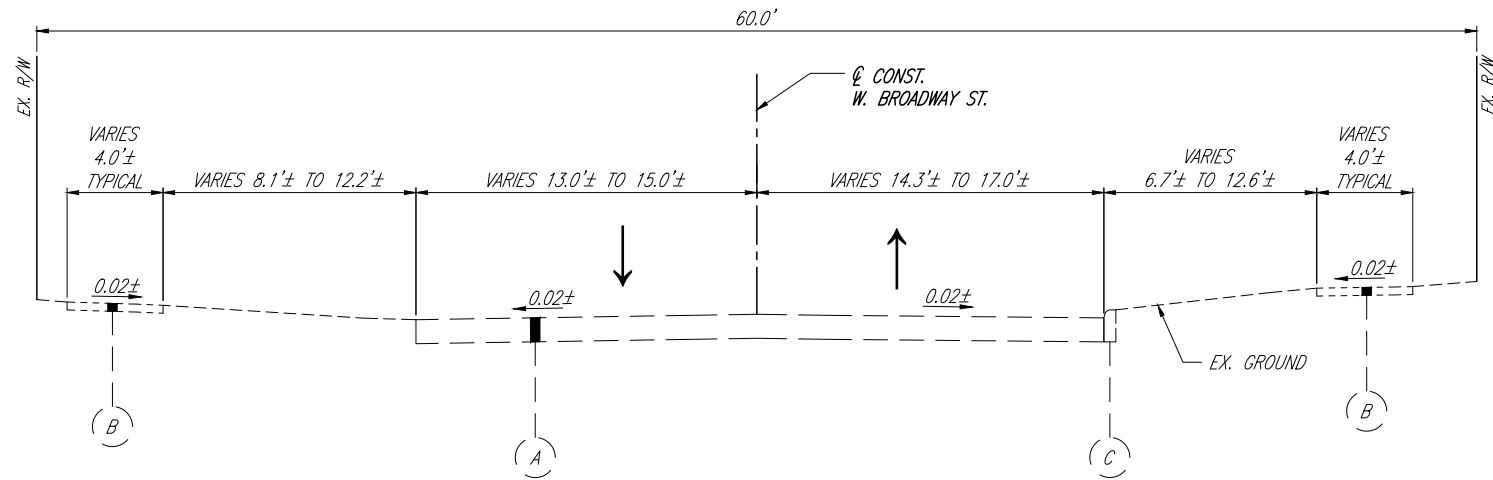
FEDERAL PROJECT NO.
N/A

RAILROAD INVOLVEMENT
NONE

VILLAGE OF PLYMOUTH
DRAINAGE IMPROVEMENTS

1
16

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ITEM LEGEND

1	254	2.0"	PAVEMENT PLANING, ASPHALT CONCRETE
2	407		NON-TRACKING TACK COAT (SEE C&MS TABLE 407.06-1 FOR APPLICATION RATES)
3	441	1.25"	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22
4	441	0.75"	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449)
5	609		CURB, TYPE 4-C, AS PER PLAN/CURB, TYPE 6, AS PER PLAN
6	659		SEEDING, MISC.: SEEDING AND MULCHING, CLASS 1
7	411		STABILIZED CRUSHED AGGREGATE (FOR GRAVEL DRIVEWAYS)
	452		6" NON-REINFORCED CONCRETE PAVEMENT (FOR CONCRETE/ASPHALT DRIVEWAYS)
	608		4" CONCRETE WALK (FOR SIDEWALKS)
8	204		SUBGRADE COMPACTION, AS PER PLAN

(A)	EXISTING ASPHALT PAVEMENT. BUILDUP AND THICKNESS UNKNOWN.
(B)	EXISTING CONCRETE WALK
(C)	EXISTING CURB (CONCRETE OR SANDSTONE) ALONG PORTIONS OF ROADWAY TO BE REMOVED
(D)	EXISTING DRIVEWAY/SIDEWALK (CONCRETE, ASPHALT, OR GRAVEL)

TYPICAL SECTION NOTES

- ALL CURB AND JOINTS BETWEEN EXISTING AND PROPOSED PAVEMENT SHALL BE SEALED PER 401.17
- ALL COSTS ASSOCIATED WITH THESE NOTES SHALL BE INCIDENTAL TO PROPOSED WORK ITEMS AND NO ADDITIONAL COMPENSATION WILL BE MADE
- A FULL DEPTH SAWCUT SHALL BE USED AT THE EDGES OF PAVEMENT TO BE REMOVED TO MINIMIZE DAMAGE TO THE EXISTING PAVEMENT TO REMAIN. THIS SAWCUT SHALL BE CONSIDERED INCIDENTAL TO THE ADJACENT CURB OR PAVEMENT ITEM TO BE REMOVED.

THE INTENT IS TO PLACE THE PROPOSED CURB AT THE EDGE OF THE EXISTING ROADWAY PAVEMENT. CURB OFFSETS HAVE BEEN PROVIDED IN THE PLANS.

THE NEW CONCRETE CURB SHALL BE A TYPE 6 CURB EVERYWHERE EXCEPT FOR THE FOLLOWING LOCATIONS WHERE A TYPE 4-C CURB IS REQUIRED.

FROM STA. 10+47.00 LT. TO STA. 19+13.52 LT.
FROM STA. 21+30.69 LT. TO STA. 21+95.87 LT.
FROM STA. 29+29.44 RT. TO STA. 30+75.50 RT.
FROM STA. 29+86.01 LT. TO STA. 30+75.50 LT.

STANDARD 2" HIGH DROP CURB AS PER ODOT SCD BP-4.1

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GENERAL

WHERE SPECIFIED, THE 2019 STATE OF OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS (C&MS) (ODOT ITEM NUMBERS) INCLUDING SUPPLEMENTAL SPECIFICATIONS (SS) AND STANDARD CONSTRUCTION DRAWINGS (SCD) SHALL APPLY EXCEPT AS MODIFIED OR EXPANDED HEREIN OR IN THE TECHNICAL SPECIFICATIONS.

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR COMPLYING WITH ALL FEDERAL, STATE, AND LOCAL SAFETY REQUIREMENTS, TOGETHER WITH EXERCISING PRECAUTIONS AT ALL TIMES FOR THE PROTECTION OF ALL PERSONS, INCLUDING EMPLOYEES, AND PROPERTY.

ALL ITEMS OF WORK CALLED FOR ON THE PLANS FOR WHICH NO SPECIFIC METHOD OF PAYMENT IS PROVIDED SHALL BE PERFORMED BY THE CONTRACTOR. THE COST OF SAID ITEM SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS RELATED ITEMS. IT IS THE INTENTION OF THE CONTRACT DOCUMENTS TO PROVIDE AND REQUIRE A COMPLETED PROJECT READY FOR OPERATION. ANY WORK ITEMS OMITTED FROM SUCH CONTRACT DOCUMENTS WHICH ARE CLEARLY NECESSARY FOR THE COMPLETION OF SUCH WORK AND ITS APPURTENANCES SHALL BE CONSIDERED A PART OF SUCH WORK ALTHOUGH NOT DIRECTLY SPECIFIED OR CALLED FOR IN THE CONTRACT DOCUMENTS.

ANY DEFECTS IN CONSTRUCTION, INCLUDING MATERIAL OR WORKMANSHIP, SHALL BE CORRECTED BY REMOVAL AND REPLACEMENT OR OTHER APPROVED METHODS PRIOR TO THE ACCEPTANCE BY THE OWNER. THE COST SHALL BE AT THE EXPENSE OF THE CONTRACTOR.

ANY MODIFICATIONS OF THE WORK AS SHOWN ON THESE APPROVED PLANS SHALL HAVE PRIOR WRITTEN APPROVAL OF THE OWNER.

ALL SURFACES INCLUDING DRAINAGE STRUCTURES, LANDSCAPING, PAVEMENTS, DRIVEWAYS, BERMS AND OTHER SURFACES DISTURBED DURING CONSTRUCTION OF THIS PROJECT SHALL BE RESTORED TO THEIR ORIGINAL CONDITION BY THE CONTRACTOR AT NO ADDITIONAL COST.

THE CONTRACTOR SHALL KEEP ALL STREETS, LANES AND PARKING AREAS ADJACENT TO THE PROJECT CLEAN AND FREE FROM ANY DEBRIS, MUD AND/OR OTHER CONSTRUCTION EQUIPMENT AT ALL TIMES DURING THE PROJECT.

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

CHARTER COMMUNICATIONS
ATTN: SEAN BEAVIS
1575 LEXINGTON AVENUE
MANSFIELD, OHIO 44907
(419) 632-6723

VILLAGE OF PLYMOUTH
ELECTRIC
ATTN: JAMES BURTON
48 W. BROADWAY ST.
PLYMOUTH, OHIO 44865
(419) 687-4331

COLUMBIA GAS OF OHIO
ATTN: TYLER HEMINGER
CC: NATE KLAIBER
1021 N. MAIN STREET
MANSFIELD, OHIO 44903
(419) 632-0599

VILLAGE OF PLYMOUTH
WATER/SEWER
ATTN: JAMES BURTON
48 W. BROADWAY ST.
PLYMOUTH, OHIO 44865
(419) 687-4331

THE OHIO UTILITIES PROTECTION SERVICE CALL BEFORE YOU DIG 811.

THE LOCATIONS OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C. THEY ARE APPROXIMATE ONLY; THE ENGINEER AND OWNER CANNOT GUARANTEE THEIR ACCURACY OR COMPLETENESS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL UTILITIES AS TO LINE AND GRADE BEFORE STARTING ANY WORK.

PROTECTION OF EXISTING UTILITIES AND PIPES

THE CONTRACTOR SHALL BE REQUIRED, AT HIS OWN EXPENSE, TO DO EVERYTHING NECESSARY TO PROTECT, SUPPORT AND SUSTAIN ALL STORM SEWERS, WATER AND GAS PIPES, SERVICE PIPES, ELECTRIC LIGHTS, POWER AND TELEPHONE POLES, CONDUIT AND OTHER FIXTURES LAID ACROSS OR ALONG THE SITE OF THE WORK. THE ENGINEER AS WELL AS THE COMPANY OR CORPORATION OWNING SAID PIPES, POLES OR CONDUITS MUST BE NOTIFIED OF THE SAME BY THE CONTRACTOR, BEFORE ANY SUCH FIXTURES ARE REMOVED OR DISTURBED. IN CASE ANY OF THE SAID SEWER, GAS OR WATER PIPES, SERVICE PIPES, ELECTRICAL LIGHT, POWER AND TELEPHONE POLES, FIBER OPTIC CABLES, CONDUITS OR OTHER FIXTURES ARE DAMAGED THEY SHALL BE REPAIRED BY THE AUTHORITIES HAVING CONTROL OF THE SAME, AND THE EXPENSE OF SAID REPAIRS SHALL BE DEDUCTED FROM THE MONIES WHICH ARE DUE OR TO BECOME DUE THE CONTRACTOR UNDER THIS CONTRACT.

SHOULD IT BECOME NECESSARY TO CHANGE THE POSITION, OR TEMPORARILY REMOVE ANY STORM SEWER, SANITARY SEWER, ELECTRIC CONDUITS, WATER PIPES, GAS PIPES, FIBER OPTIC LINES, OR OTHER PIPES OR WIRES IN ORDER TO PERMIT THE CONTRACTOR TO USE A PARTICULAR METHOD OF CONSTRUCTION OR IN ORDER TO CLEAR THE STRUCTURES BEING BUILT, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF THE LOCATION AND CIRCUMSTANCES AND SHALL CEASE WORK, IF NECESSARY, UNTIL SATISFACTORY ARRANGEMENTS HAVE BEEN MADE BY THE OWNERS OF SAID PIPES OR WIRES TO PROPERLY CARE FOR THE SAME. THE ENTIRE COST OF THE CHANGES OR TEMPORARY REMOVAL MUST BE INCLUDED IN THE PRICES STIPULATED FOR THE VARIOUS ITEMS OF WORK TO BE DONE UNDER THIS CONTRACT.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROTECTION OF RIGHT-OF-WAY

CONSTRUCT ALL ACTIVITIES, EQUIPMENT STORAGE, AND STAGING TO WITHIN THE CONSTRUCTION LIMITS AND THE EXISTING RIGHT-OF-WAY.

SUBMIT A WRITTEN REQUEST TO THE PROJECT ENGINEER TO USE ANY AREA OUTSIDE THESE LIMITS. THE DOCUMENT SUBMITTED MUST CLEARLY IDENTIFY THE AREA AND EXPLAIN THE PROPOSED USE AND RESTORATION OF THE AREA. USE OF THESE AREAS FOR DISPOSAL OF WASTE MATERIAL AND CONSTRUCTION DEBRIS, EXCAVATION OF BORROW MATERIAL AND PLACEMENT OF PORTABLE PLANTS IS PROHIBITED. THE REQUEST MUST BE APPROVED, IN WRITING, BEFORE THE CONTRACTOR HAS PERMISSION TO USE THE AREA.

ANY ITEMS DAMAGED BEYOND THE CONSTRUCTION LIMITS AS DEFINED ABOVE WILL BE REPLACED IN KIND OR AS APPROVED BY THE PROJECT ENGINEER.

PROTECTION OF MONUMENTS, PROPERTY CORNER MARKERS, ETC.

MONUMENTS, PROPERTY CORNER MARKERS, ETC., SHALL NOT BE DISTURBED BY THE CONTRACTOR. IN THE EVENT IT IS NECESSARY TO REMOVE THESE ITEMS, THE CONTRACTOR SHALL AT HIS EXPENSE, EMPLOY A REGISTERED SURVEYOR TO PROPERLY REFERENCE THE POINTS AND SHALL RESET SAME AFTER CONSTRUCTION HAS BEEN COMPLETED IN THE AREA.

PRECONSTRUCTION VIDEO

THE CONTRACTOR SHALL SUBMIT A PRECONSTRUCTION VIDEO OF THE ENTIRE PROJECT'S WORK LIMITS TO THE ENGINEER PRIOR TO COMMENCING WORK. COSTS SHALL BE INCLUDED IN ITEM 624 – MOBILIZATION AND NO SEPARATE PAYMENT WILL BE MADE FOR SAID WORK.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER.

VERTICAL CURB TAPERS

ALL VERTICAL CURB TAPERS SHALL BE 2" TO 4" OR 6" (BY CURB TYPE) IN 10' AND 4" TO 6" IN 10' UNLESS OTHERWISE APPROVED BY THE ENGINEER TO BETTER FIT FIELD CONDITIONS OR AS INDICATED IN THESE PLANS.

ITEM 201 – CLEARING AND GRUBBING, AS PER PLAN

THIS ITEM SHALL INCLUDE ALL CLEARING, GRUBBING, SCALPING AND THE REMOVAL AND SATISFACTORY DISPOSAL OF TREES, STUMPS, VEGETATION AND DEBRIS AS MAY BE NECESSARY FOR THE COMPLETION OF THE WORK. THIS ITEM SHALL ALSO INCLUDE THE REMOVAL OF MISCELLANEOUS ITEMS CALLED OUT TO BE REMOVED IN THE PLANS (STONE WALL, RISER, ROCKS, ETC.) THAT ARE NOT SEPARATELY ITEMIZED.

SIZE	NO. TREES	NO. STUMPS	TOTAL
N/A	0	1	1

THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT AND SAVE ALL TREES WHICH ARE ADJACENT TO THE LINE OF WORK AND SHALL REMOVE ONLY THOSE TREES WHICH ARE DESIGNATED FOR REMOVAL ON THE PLANS OR DIRECTED BY THE ENGINEER. TREE ROOTS AND OVERHANGING BRANCHES SHALL NOT BE CUT EXCEPT WITH SPECIAL PERMISSION OF THE ENGINEER. WHEN REQUIRED, THE CUTTING OF ROOTS AND BRANCHES SHALL BE NEATLY TRIMMED AND COVERED WITH GRAFTING WAX. ALL DAMAGE SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE ENGINEER.

ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

NO ADDITIONAL PAYMENT WILL BE MADE FOR THE TREES REQUIRED TO BE REMOVED AND NOT SPECIFICALLY MARKED ON THE PLANS. NO TREES SHALL BE REMOVED WITHOUT PERMISSION FROM THE ENGINEER.

PAYMENT FOR ALL OPERATIONS FOR TREE AND STUMP REMOVAL SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

EROSION CONTROL

EROSION, RESULTING FROM CONSTRUCTION ACTIVITIES, SHALL BE MINIMIZED AND SEDIMENT, RESULTING FROM CONSTRUCTION ACTIVITIES, SHALL NOT LEAVE THE PROJECT LIMITS. ANY BMP'S NOT SPECIFICALLY COVERED OR DISCUSSED IN SS 832 OR SCD DM-4.4, SHALL BE APPROVED BY THE ENGINEER, PRIOR TO USE ON THE PROJECT. THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 832 – EROSION CONTROL	10,000 EA
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SAWCUTS

ALL REQUIRED SAWCUTS ON THE PROJECT SHALL BE INCIDENTAL TO ASSOCIATED PROJECT ITEMS. NO SEPARATE PAYMENTS WILL BE MADE.

EXCAVATION AND EMBANKMENT

C&MS ITEM 203 EXCAVATION AND C&MS ITEM 203 EMBANKMENT HAVE NOT BEEN SEPARATELY ITEMIZED IN THESE PLANS. ALL NECESSARY EXCAVATION AND EMBANKMENT SHALL BE INCIDENTAL TO THE ITEMS DEFINED FOR CONSTRUCTION IN THESE PLANS AND NO ADDITIONAL COMPENSATION SHALL BE MADE FOR THE REMOVAL AND DISPOSAL OF C&MS ITEM 203 EXCAVATION OR THE FURNISHING, PLACING, AND COMPACTION OF C&MS ITEM 203 EMBANKMENT. ALL EXCAVATION AND EMBANKMENT WORK SHALL BE PERFORMED IN ACCORDANCE WITH C&MS ITEM 203.

ALL EXCESS EXCAVATION SURPLUS MATERIAL AND RUBBISH SHALL BE DISPOSED OF BY CONTRACTOR PER C&MS 203.04, 105.16 & 105.17.

ITEM 659 – SEEDING, MISC.: SEEDING AND MULCHING, CLASS 1

SEEDING AND MULCHING (CLASS 1) SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT. THIS ITEM SHALL ALSO INCLUDE ALL REPAIR SEEDING AND MULCHING, INTER-SEEDING, LIME, COMMERCIAL FERTILIZER AND WATER PER C&MS 659 REQUIRED TO REPAIR ALL DAMAGE OR EROSION OF THE SEEDED AND MULCHED AREAS BEFORE THE COMPLETION OF THE PROJECT. PAYMENT FOR THIS ITEM SHALL BE MADE AT THE LUMP SUM PRICE BID FOR ITEM 659 – SEEDING, MISC.: SEEDING AND MULCHING, CLASS 1.

ITEM 202 – PAVEMENT REMOVED, AS PER PLAN

THIS ITEM SHALL INCLUDE THE REMOVAL AND DISPOSAL OF ALL MATERIALS INCLUDING, BUT NOT LIMITED TO, ASPHALT, BRICK, CONCRETE, STONE ETC. WITHIN THE LIMITS OF THE PROPOSED STORM SEWER WORK. THE COST FOR SAWCUTTING AND REMOVAL OF PAVEMENT HAS BEEN INCLUDED WITH THE PERTINENT 611 ITEM.

ITEM 202 – CURB REMOVED, AS PER PLAN

THE EXISTING CURB WITHIN THE PROJECT LIMITS IS SPORADIC AND MAY BE COVERED BY EXISTING PAVEMENT OR EARTH. DUE TO THIS AN ACCURATE QUANTITY OF CURB REMOVAL COULD NOT BE ESTABLISHED. THIS ITEM SHALL INCLUDE THE REMOVAL AND DISPOSAL OF ALL CONCRETE/SANDSTONE CURB WITHIN THE LIMITS OF THE PROPOSED STORM SEWER AND CURB WORK. THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 202 – CURB REMOVED, AS PER PLAN	100 FT
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ITEM 411 – STABILIZED CRUSHED AGGREGATE, AS PER PLAN

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER TO TOP DRESS EXISTING AGGREGATE DRIVEWAYS AND PARKING AREAS UPON COMPLETION OF CURB WORK. PROPOSED MATERIAL TYPE SHALL RELATIVELY MATCH THE EXISTING DRIVEWAY MATERIAL TYPE. QUANTITIES SHALL NOT BE SEPARATED AND ARE NOT ITEMIZED BY MATERIAL TYPE. ALL MATERIAL TYPES SHALL BE COMPENSATED UNDER THIS ITEM. THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 411 – STABILIZED CRUSHED AGGREGATE, AS PER PLAN	5 CY
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ITEM 204 – SUBGRADE COMPACTION, AS PER PLAN

THERE ARE NUMEROUS UNDERGROUND UTILITIES THAT ARE TO REMAIN IN PLACE DURING CONSTRUCTION OF THE STORM SEWER, REPAIR PAVEMENT, AND CURB THAT WILL BE DIRECTLY BENEATH THE PROPOSED WORK. DUE TO THIS ONLY A STATIC ROLLER SHALL BE USED FOR THE COMPACTION OF ALL NORMAL AND STABILIZED SUBGRADES. ANY DAMAGE CAUSED TO EXISTING UNDERGROUND UTILITIES DUE TO THE USE OF UNAPPROVED COMPACTION PROCEDURES WILL BE REPAIRED AT THE EXPENSE OF THE CONTRACTOR.

PAVEMENT RESTORATION FOR PIPE AND DRAINAGE STRUCTURES INSTALLATIONS AND/OR REMOVALS

THE FOLLOWING ITEMS SHALL BE PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION AND/OR REMOVAL OF PIPES AND DRAINAGE STRUCTURES.

ITEM 202 – PAVEMENT REMOVED, AS PER PLAN	675 SY
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ASPHALT	
ITEM 204 – SUBGRADE COMPACTION, AS PER PLAN	675 SY
ITEM 301 – 5" ASPHALT CONCRETE BASE, PG64-22 (449)	94 CY
ITEM 441 – 3" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (449)	57 CY

THE ABOVE ITEM LIST IS BASED ON THE TRENCH REPAIR DETAILS SHOWN ON SHEET Z AND A PAVEMENT RESTORATION WIDTH THAT SHALL INCLUDE THE TRENCH WIDTH PLUS ONE FOOT ON EACH SIDE OF THE TRENCH AND A WIDTH OF TWO FEET AROUND THE PERIMETER OF THE PROPOSED DRAINAGE STRUCTURE OR THE REMOVAL LIMITS OF AN EXISTING DRAINAGE STRUCTURE.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

PROFILE AND ALIGNMENT

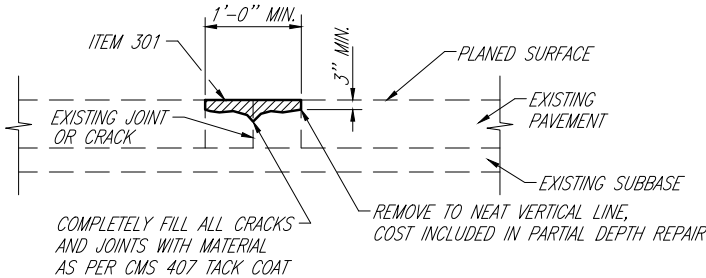
PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY WITH A UNIFORM TOTAL THICKNESS OF 2" AS SHOWN ON THE TYPICAL SECTIONS.

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ITEM 253 – PAVEMENT REPAIR

THIS ITEM SHALL CONSIST OF REMOVAL AND REPLACEMENT OF EXISTING ASPHALT PAVEMENT IN ACCORDANCE WITH ITEM 253. THE EXISTING PAVEMENT SHALL BE REMOVED TO A DEPTH OF 3 INCHES AND REPLACED WITH ITEM 301 ASPHALT CONCRETE BASE, PG64–22 (449) MATERIAL. PAVEMENT REPAIRS SHALL BE COMPLETED AFTER PAVEMENT PLANING OPERATION.

THE ITEM SHALL INCLUDE ALL MATERIALS, LABOR AND TOOLS NECESSARY TO REPAIR AREAS OF DETERIORATED ASPHALT PAVEMENT IN ACCORDANCE WITH CMS 253 AND THE FOLLOWING TYPICAL DETAIL.



THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER FOR PAVEMENT REPAIRS AS PER THE DETAIL ABOVE:

ITEM 253, PAVEMENT REPAIR 60 CY

ITEM 609 – CURB, TYPE 4-C, AS PER PLAN

ITEM 609 – CURB, TYPE 6, AS PER PLAN

THIS ITEM SHALL CONFORM TO C&MS 609 WITH THE EXCEPTION THE PROPOSED CURB SHALL BE PLACED AGAINST THE EXISTING PAVEMENT AND ANY ADDITIONAL CONCRETE NECESSARY TO FILL VOIDS BETWEEN THE EXISTING PAVEMENT AND THE FACE OF THE PROPOSED CURB SHALL BE CONSIDERED INCIDENTAL TO THIS ITEM. THE ADDITIONAL CONCRETE SHALL EXTEND FROM THE TOP OF THE EXISTING PAVEMENT TO THE BOTTOM OF THE PROPOSED CURB.

THIS ITEM SHALL ALSO INCLUDE THE EARTHWORK (EXCAVATION AND EMBANKMENT) NECESSARY TO PREPARE AND SHAPE THE EXISTING GROUND BENEATH THE PROPOSED CURB AND PERFORM ANY EXCAVATION OR EMBANKMENT TO SLOPE THE FINAL GRADE ADJACENT TO THE WALK AND CURB IN A MANNER TO MAINTAIN POSITIVE DRAINAGE THROUGHOUT THE PROJECT. ALL COST FOR EXCAVATION AND EMBANKMENT NECESSARY TO PERFORM THIS GRADING WORK TO THE SATISFACTION OF THE ENGINEER INCLUDING ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT COST FOR THIS ITEM.

PAYMENT FOR THIS ITEM SHALL BE MADE AT THE UNIT CONTRACT BID PRICE PER FOOT FOR ITEM 609–CURB, TYPE 4–C, AS PER PLAN, AND ITEM 609–CURB, TYPE 6, AS PER PLAN, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO CONSTRUCT THIS ITEM TO THE SATISFACTION OF THE ENGINEER.

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE VILLAGE, REPRESENTATIVES OF THE CONTRACTOR AND THE VILLAGE OF PLYMOUTH SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE VILLAGE.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE VILLAGE.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEM.

ITEM 202 – REMOVAL MISC.: PLUG AND ABANDON EXISTING CONDUIT

THIS ITEM SHALL CONSIST OF THE CONSTRUCTION OF BULKHEADS AT THE ENDS OF THE EXISTING CONDUIT TO BE ABANDONED IN PLACE. BULKHEADS SHALL BE LOCATED AT THE LIMITS OF THE AREA TO BE ABANDONED AS INDICATED ON THE PLANS. THE BULKHEADS SHALL CONSIST OF BRICK OR CONCRETE MASONRY WITH A MINIMUM THICKNESS OF 12 INCHES.

THE COST FOR CAPPING THE ENDS OF THE EXISTING CONDUITS TO BE ABANDONED SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS TO COMPLETE THE ITEM TO THE SATISFACTION OF THE ENGINEER AND NO ADDITIONAL COMPENSATION SHALL BE MADE. PAYMENT SHALL BE MADE AT THE UNIT PRICE BID PER EACH ITEM 202 – REMOVAL MISC.: PLUG AND ABANDON EXISTING CONDUIT.

ITEM 611 – PIPE CULVERTS, SEWERS, DRAINS, AND DRAINAGE STRUCTURES, AS PER PLAN

ALL STORM SEWERS AND DRAINAGE STRUCTURES SHALL BE INSTALLED PER ODOT ITEM 611, THE TYPICAL TRENCH DETAIL, AND MANUFACTURERS' RECOMMENDATIONS. THE MOST STRINGENT SHALL APPLY. THE FOLLOWING ODOT 611 SPECIFICATION SECTIONS DO NOT APPLY TO THIS PROJECT: 611.04.D. (PERFORMANCE REPORT), 611.12, 611.13, 611.14, & 611.15.

IN ADDITION ALL PAVEMENT RESTORATION/ REPAIR AS INDICATED IN THESE NOTES SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PERTINENT 611 DRAINAGE ITEM.

ITEM 611 – CATCH BASIN (GRATES GENERAL NOTE)

ALL CATCH BASINS SHALL BE INSTALLED WITH BICYCLE SAFE GRATES. COSTS FOR GRATES SHALL BE INCLUDED IN THE ASSOCIATED CATCH BASIN ITEMS INCLUDING RECONSTRUCTION AND ADJUSTMENT TO GRADE.

ITEM 611 – CATCH BASIN, NO. 3, AS PER PLAN

THIS ITEM SHALL CONFORM TO C&MS SECTION 611 AND SCD CB–3. GRATES SUPPLIED SHALL BE BICYCLE SAFE. THE CONCRETE APRON SHALL BE 2 FEET AROUND ALL SIDES OF THE CATCH BASIN IN PAVEMENT. THIS ITEM SHALL ALSO INCLUDE THE CURB PLACED INTEGRAL WITH THE APRON. THE CURB HEIGHT SHALL BE TRANSITIONED FROM 4" AT THE CURB INLET OPENING TO MATCH THE EXISTING CURB HEIGHT OVER THE 2 FOOT APRON. THE MINIMUM TRANSITION CURB HEIGHT SHALL BE A 1" REVEAL.

PAYMENT FOR THIS ITEM SHALL BE MADE AT THE UNIT CONTRACT PRICE BID PER EACH FOR ITEM 611 – CATCH BASIN, NO. 3, AS PER PLAN AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO CONSTRUCT THIS ITEM TO THE SATISFACTION OF THE ENGINEER.

ITEM 611 – CATCH BASIN, NO. 3A, AS PER PLAN

THIS ITEM SHALL CONFORM TO C&MS 611 AND STANDARD CONSTRUCTION DRAWING CB–3A WITH THE EXCEPTION THAT THE CURB HEIGHT SHALL BE 4" IN SOME LOCATIONS. THE GRATE SUPPLIED SHALL ALSO BE BICYCLE SAFE.

ITEM 611 – MANHOLE ADJUSTED TO GRADE, AS PER PLAN

THIS ITEM SHALL CONFORM TO C&MS 611 WITH THE EXCEPTION THAT A 12" CONCRETE COLLAR SHALL BE PLACED AROUND THE MANHOLE COVER CASTING.

ITEM 611 – INLET SIDE DITCH, AS PER PLAN

THIS ITEM SHALL CONFORM TO C&MS 611, STANDARD CONSTRUCTION DRAWING CB–1, AND THE YARD DRAIN – NON TRAFFIC DETAIL ON SHEET 8. PIPE SIZES AND MATERIALS SHALL BE AS SPECIFIED IN THE PLANS.

ITEM 638 – VALVE BOX ADJUSTED TO GRADE, AS PER PLAN

THIS ITEM SHALL CONFORM TO C&MS 638 WITH THE EXCEPTION THAT A MINIMUM 12–INCH WIDE CONCRETE COLLAR SHALL BE PLACED AROUND THE VALVE BOX.

ITEM 638 – WATER WORK, MISC.: LOWER WATER SERVICE CONNECTION

THIS ITEM SHALL CONFORM TO C&MS 638 AND THE WATER SERVICE LINE LOWERING DETAIL ON SHEET 8 OF THESE PLANS. THIS ITEM SHALL INCLUDE ALL THE COST FOR CONNECTIONS, BENDS, WATER LINE, ETC. TO COMPLETE THE WATER SERVICE LINE LOWERING AT THE LOCATIONS INDICATED IN THESE PLANS. PAYMENT FOR THIS ITEM SHALL BE MADE AT THE UNIT CONTRACT BID PRICE PER EACH AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS FOR ITEM 638 – WATER WORK, MISC.: LOWER WATER SERVICE CONNECTION. NO ADDITIONAL COMPENSATION SHALL BE MADE FOR THE WORK INVOLVED IN LOWERING THE WATER SERVICE LINE.

ITEM 638 – WATER WORK, MISC.: LOWERING OF WATER LINE (8")

THIS ITEM SHALL CONFORM TO C&MS 638 AND THE WATER MAIN LOWERING DETAIL ON SHEET 8 OF THESE PLANS. THIS ITEM SHALL INCLUDE ALL THE COST FOR CONNECTIONS, BENDS, WATER LINE, ETC. TO COMPLETE THE WATER MAIN LOWERING AT THE LOCATIONS INDICATED IN THESE PLANS FOR 8" WATER MAIN AND A TOTAL LENGTH OF 40'. PAYMENT FOR THIS ITEM SHALL BE MADE AT THE UNIT CONTRACT BID PRICE PER EACH AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS FOR ITEM 638 – WATER WORK, MISC.: LOWERING OF WATER LINE (8"). NO ADDITIONAL COMPENSATION SHALL BE MADE FOR THE WORK INVOLVED IN LOWERING THE WATER MAIN.

LSMB–MINIMUM CLEARANCE

LOW STRENGTH MORTAR BACKFILL (LSMB) CONFORMING TO C&MS 613 IS TO BE PLACED IN LOCATIONS WHERE A MINIMUM CLEARANCE OF 18 INCHES CANNOT BE MAINTAINED BETWEEN THE PROPOSED STORM SEWER AND OTHER EXISTING UTILITIES (I.E. WATERLINE, SANITARY SEWERS, GAS LINES, ETC.) AS SHOWN IN THE TYPICAL UTILITY CROSSING REPLACEMENT DETAIL ON SHEET Z. LSMB SHALL EXTEND 12 INCHES ON EITHER SIDE OF THE UTILITY CROSSING IN THE DIRECTION OF THE TRENCH. SEE LSMB DETAIL ON SHEET Z. PAYMENT FOR LSMB SHALL BE INCLUDED IN THE UNIT BID PRICE OF 611 CONDUIT ITEMS, SEPARATE PAYMENT WILL NOT BE MADE.

LSMB–MINIMUM COVER

LOW STRENGTH MORTAR BACKFILL (LSMB) CONFORMING TO C&MS 613 IS TO BE PLACED IN LOCATIONS WHERE A MINIMUM COVER OF 24 INCHES CANNOT BE MAINTAINED ABOVE THE PROPOSED STORM SEWER. LSMB SHALL EXTEND A MIN. 12 INCHES PAST THE PIPE O.D. ON EACH SIDE OF THE TRENCH. FILL THE TRENCH WITH LSMB TO WITHIN 8 INCHES OF FINISH GRADE. THE REMAINING 8 INCHES SHALL BE FILLED WITH TOPSOIL. PAYMENT FOR LSMB SHALL BE INCLUDED IN THE UNIT BID PRICE OF 611 CONDUIT ITEMS, SEPARATE PAYMENT WILL NOT BE MADE. SEE LSMB DETAIL ON SHEET Z.

WATER MAIN AND SERVICE LINE REPAIR REQUIREMENTS:

1. WATER MAIN SHALL BE ODOT ITEM 748.02, POLYVINYL CHLORIDE (P.V.C.) PRESSURE CLASS 235. PIPE SHALL CONFORM TO AWWA C–909. PIPE JOINTS SHALL BE PUSH–ON TYPE WITH A THICKENED BELL AND SHALL CONFORM TO ASTM D3139 WITH RUBBER GASKET CONFORMING TO ASTM F477.
2. FITTINGS SHALL BE DUCTILE IRON EQUAL TO ANSI/AWWA A21.53/C153 LATEST REVISIONS. JOINTS SHALL CONFORM WITH ANSI/AWWA A21.11/C111 LATEST REVISION FOR MECHANICAL JOINTS.
3. TRENCHES AND INSTALLATION. TRENCHES FOR WATER MAINS SHALL BE EXCAVATED AND BACKFILLED IN CONFORMANCE WITH ODOT 638. P.V.C. PIPE SHALL BE TOTALLY ENCASED IN AGGREGATE. A TRACING TAPE MARKED "BURIED WATERLINE BELOW" SHALL BE PLACED 1 FOOT BELOW GROUND SURFACE OVER THE FULL LENGTH OF ALL P.V.C. WATER MAIN.
4. HYDRANTS SHALL BE AWWA C502, MODEL A423 SUPER CENTURION AS MANUFACTURED BY, "MUELLER COMPANY", NATIONAL STANDARD THREAD, VILLAGE OF PLYMOUTH STANDARD, COUNTERCLOCKWISE OPENING, AND PAINTED RED.
5. GATE VALVES SHALL BE AWWA C509, MUELLER RESILIENT WEDGE A–2360 WITH STAINLESS STEEL FASTENERS, COUNTERCLOCKWISE OPENING, VILLAGE OF PLYMOUTH STANDARD OR APPROVED EQUAL. VALVE BOX SHALL BE CAST IRON, BUFFALO TYPE, TWO–PIECE, FIVE AND ONE–QUARTER INCH SCREW TYPE SHAFT WITH COVER TO FIT VALVE, ALL PER VILLAGE OF PLYMOUTH STANDARD.
6. HYDROSTATIC TESTING SHALL BE PER O.D.O.T. ITEM 638.09, AWWA C605, 150 PSI TEST PRESSURE PERFORMED BY THE CONTRACTOR.
7. SERVICE BRANCHES SHALL BE ULTRA HIGH MOLECULAR WEIGHT POLYETHYLENE TUBING IN ACCORDANCE WITH, ASTM D–2737, PE 3408, D–1238, SDR–9 (200 PSI PRESSURE CLASS) COMPRESSION FITTINGS PER AWWA C800. CORPORATION STOPS SHALL BE MUELLER COMPANY H–15008 WITH S.S. PIPE LINERS OR APPROVED EQUAL. CURB BOXES SHALL BE MUELLER COMPANY H–10306, SIZE 94E. COVERS SHALL HAVE "WATER" CAST IN RAISED LETTERS, FASTENED BY A BRASS PENTAGON PLUG. CURB STOPS SHALL BE MUELLER COMPANY H–15209 WITH S.S. PIPE LINERS OR APPROVED EQUAL. SERVICE SADDLES SHALL BE ROMAC INDUSTRIES STYLE 306, MUELLER SS SERIES, OR APPROVED EQUAL.
8. EXISTING WATER VALVES SHALL ONLY BE OPERATED BY VILLAGE OF PLYMOUTH PERSONNEL.
9. VALVE BOXES INSTALLED IN ASPHALT PAVEMENT SHALL BE SET IN A MINIMUM 12 INCH WIDE CONCRETE COLLAR WITH A THICKNESS EQUAL TO THE PAVEMENT SECTION AND FINISHED 1/4 INCH BELOW THE PAVEMENT SURFACE. SEAL THE CONCRETE/ASPHALT INTERFACE WITH ITEM 702.01 LIQUID ASPHALT BINDER.
10. ALL PROPOSED GATE VALVES SHOWN IN PLAN AND PROFILE SHALL INCLUDE BOX & COVER.
11. FIRE HYDRANTS SHOWN ON THE PLANS TO BE "REMOVED FOR STORAGE" SHALL BE REMOVED & STORED AT THE VILLAGE OF PLYMOUTH WATER DEPARTMENT YARD.
12. WATER MAIN EMBEDMENT MATERIAL SHALL BE ODOT 703.11, TYPE 3 (ASTM NO. 57). SLAG IS NOT ALLOWABLE.

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RESIDENTIAL DRAINAGE CONNECTIONS

ALL RESIDENTIAL DRAINS, WHICH ARE ENCOUNTERED DURING CONSTRUCTION, SHALL BE PROVIDED WITH UNOBSTRUCTED OUTLETS. EXISTING COLLECTORS, ISOLATED FARM DRAINS, ROOF DRAINS, FOOTER DRAINS OR YARD DRAINS WHICH ARE ENCOUNTERED ABOVE THE ELEVATION OF ROADWAY SHALL BE OUTLETTED INTO THE ROADWAY. EXISTING DRAINS ENCOUNTERED THAT TIE INTO THE EXISTING STORM SEWER SHALL BE TIED BACK INTO THE PROPOSED STORM SEWER SYSTEM AT THE SAME LOCATION AS THE EXISTING TIE-IN (UNLESS OTHERWISE NOTED IN THE PLANS). ALL DRAINAGE CONNECTIONS SHALL USE 611 TYPE F CONDUIT.

THE LOCATION, TYPE, SIZE AND GRADE OF REPLACEMENTS SHALL BE DETERMINED BY THE ENGINEER AND PAYMENT SHALL BE MADE ON FINAL MEASUREMENTS. ALL TIE-INS SHALL BE DOCUMENTED AND LOCATIONS PROVIDED TO THE VILLAGE.

EROSION CONTROL PADS AND ANIMAL GUARDS SHALL BE PROVIDED AT THE OUTLET END OF ALL FARM DRAINS AS PER STANDARD CONSTRUCTION DRAWING DM-1.1, EXCEPT WHEN THEY OUTLET INTO A DRAINAGE STRUCTURE. PAYMENT FOR THE EROSION CONTROL PADS AND ANIMAL GUARDS AND ANY NECESSARY BENDS OR BRANCHES SHALL BE INCLUDED FOR PAYMENT IN THE PERTINENT CONDUIT ITEM.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR THE WORK NOTED ABOVE:

ITEM 611	4" THRU 8" CONDUIT, TYPE E	<u>50</u> FT
ITEM 611	4" THRU 8" CONDUIT, TYPE F	<u>20</u> FT

ELEVATION DATUM AND HORIZONTAL CONTROL

ALL ELEVATIONS ARE ORTHOMETRIC HEIGHTS USING THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 88) AND THE GEOID12 GEOID. HORIZONTAL POSITIONS ARE BASED ON THE OHIO STATE PLANE NORTH ZONE, A LAMBERT CONFORMAL CONIC MAP PROJECTION, THE NORTH AMERICAN DATUM OF 1983 (NAD 83 (2011)), AND THE GRS80 ELLIPSOID.

PROJECT CONTROL AND CENTERLINE TABLE							
POINT NO.	STA.	OFFSET	CL	NORTHING (Y)	EASTING (X)	ELEVATION (Z)	DESCRIPTION
BM#1	11+22.10	19.26 RT	W BROADWAY ST	484084.98	1920117.83	1027.04	CHISELED "X" ON FRONT FLANGE BOLT OF FIRE HYDRANT
BM#2	18+56.49	18.78 RT	W BROADWAY ST	484078.57	1920852.37	1012.60	CHISELED "X" ON FRONT FLANGE BOLT OF FIRE HYDRANT
CNPT#1	10+97.30	15.96 LT	W BROADWAY ST	484120.55	1920093.54	1025.08	IRON PIN SET
CNPT#2	30+26.76	15.17 RT	W BROADWAY ST	484069.74	1922022.49	1021.73	MAG NAIL SET
CNPT#3	17+67.04	28.95 LT	W BROADWAY ST	484126.81	1920763.29	1011.51	IRON PIN SET
PI	10+00.00	CL	W BROADWAY ST	484106.00	1919996.02		PI CL CONST W BROADWAY ST
PI	11+94.98	CL	W BROADWAY ST	484103.18	1920190.98		PI CL CONST W BROADWAY ST
PI	24+33.56	CL	W BROADWAY ST	484091.66	1921429.51		PI CL CONST W BROADWAY ST INTERSECTION WITH FRANKLIN ST
PI	29+12.97	CL	W BROADWAY ST	484086.21	1921908.88		PI CL CONST W BROADWAY ST INTERSECTION WITH BELL ST
PI	31+98.92	CL	W BROADWAY ST	484082.94	1922194.82		PI CL CONST W BROADWAY ST INTERSECTION WITH RAILROAD ST

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ITEM 614- MAINTAINING TRAFFIC, AS PER PLAN

GENERAL

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING AND CONTROLLING TRAFFIC ON ALL STREETS AND ROADS AFFECTED BY CONSTRUCTION, AND SHALL, PRIOR TO CONSTRUCTION, SUBMIT A CONSTRUCTION SCHEDULE AND A PLAN OF PROPOSED MAINTENANCE OF TRAFFIC OPERATIONS TO THE VILLAGE OF PLYMOUTH AND ENGINEER FOR APPROVAL INDICATING DATES AND DURATION OF EACH STAGE/PHASE OF CONSTRUCTION. THE CONTRACTOR SHALL NOTIFY THE VILLAGE OF PLYMOUTH IN WRITING A MINIMUM OF 14 DAYS IN ADVANCE OF THE FIRST ANTICIPATED MAINTENANCE OF TRAFFIC OPERATION.

VEHICULAR AND PEDESTRIAN ACCESS

THERE ARE SIDEROADS, AND RESIDENCES WITHIN THE PROJECT LIMITS THAT SHALL HAVE ACCESS MAINTAINED AT ALL TIMES, UNLESS OTHERWISE STATED IN THIS NOTE. THE CONTRACTOR SHALL GIVE 7 DAY NOTICE (IN WRITING) TO PROPERTY OWNERS BEFORE CLOSING DRIVEWAYS OR RESTRICTING ACCESS AT ANY TIME. NO DRIVEWAYS OR ACCESS POINTS SHALL BE COMPLETELY CLOSED UNTIL ACCESS AND PARKING ARRANGEMENTS HAVE BEEN MADE WITH THE PROPERTY OWNER. THE CONTRACTOR'S WRITTEN NOTICE SHALL INCLUDE CONTACT INFORMATION (CELL PHONE NUMBER) FOR THE ONSITE SUPERVISOR FOR THE CONTRACTOR SO THAT ARRANGEMENTS AND COORDINATION CAN BE EASILY COMMUNICATED.

STEEL PLATES AND/OR AGGREGATE SHALL BE USED FOR MAINTAINING VEHICULAR ACCESS. STEEL PLATES, WOOD, AND/OR OTHER ADA APPROVED HARD SURFACE TREATMENT TREATMENT SHALL BE USED FOR MAINTAINING PEDESTRIAN TRAFFIC.

ALL WORK INCLUDING LABOR, EQUIPMENT, INCIDENTALS AND MATERIALS REQUIRED TO MAINTAIN BOTH VEHICULAR AND PEDESTRIAN ACCESS AS STATED ABOVE SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM - 614 MAINTAINING TRAFFIC, AS PER PLAN.

ANY ADDITIONAL WORK INCLUDING LABOR, EQUIPMENT, INCIDENTALS, AND MATERIALS NOT SPECIFICALLY ITEMIZED BUT REQUIRED TO MAINTAIN BOTH VEHICULAR AND PEDESTRIAN ACCESS AT ALL TIMES, AS STATED ABOVE, OR AS DIRECTED BY THE ENGINEER SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM - 614 MAINTAINING TRAFFIC, AS PER PLAN.

ROADWAY WORKZONE

TWO-WAY TRAFFIC (ONE LANE IN EACH DIRECTION) SHALL BE MAINTAINED THROUGH THE PROJECT SITE, UNLESS OTHERWISE APPROVED BY THE ENGINEER. ANY NECESSARY LANE CLOSURES SHALL BE IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWINGS MT-95.31 AND MT-97.10. THE CONTRACTOR SHALL MAINTAIN A MINIMUM LANE WIDTH OF TEN (10) FEET AT ALL TIMES. FIGURE 6H-6 (TA-6) OF SECTION 6 OF THE MUTCD IS CITED AS AN APPROPRIATE EXAMPLE OF MAINLINE WORKZONES.

DROP OFFS WITHIN THE WORKZONE SHALL BE IN ACCORDANCE TO MT-101.90. TRENCHES THAT CROSS ROADWAYS, ALLEYS, AND DRIVEWAYS SHALL BE PROTECTED WITH SECURELY PINNED AND WEDGED STEEL PLATES OR BACKFILLED TO THE TOP AT THE END OF EACH WORK DAY.

ITEM 614- MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)

ROADWAY WORKZONE (CONTINUED)

THE LENGTH AND DURATION OF LANE AND ROAD CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ABSOLUTELY, UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR CLOSE A ROAD OR LANE WITHOUT PRIOR APPROVAL.

ALL WORK PERFORMED ALONGSIDE THE EXISTING ROADWAY SHALL HAVE SIGNS AND CONES AND SHALL FOLLOW ODOT STANDARD DRAWINGS AND ITEM 614.

WHENEVER THE CONTRACTOR'S WORK RESTRICTS SIGHT DISTANCE OR MANEUVERABILITY SUCH THAT THE EXISTING TRAFFIC CONTROL IS NOT SAFE, IT SHALL BE HIS/HER RESPONSIBILITY TO PROVIDE A FLAGGER TO SAFELY DIRECT TRAFFIC THROUGH THE AREA. COORDINATION IN THIS MATTER SHALL BE MADE WITH THE VILLAGE OF PLYMOUTH AND THE ENGINEER, AND ANY COSTS SHALL BE INCLUDED IN THE BID ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

PEDESTRIAN WORKZONE

THE CONTRACTOR SHALL PROVIDE TEMPORARY BARRICADES, STEEL PLATES, OR OTHER ACCEPTABLE MEANS ON THE SIDES OF THE PROPOSED CONSTRUCTION TO ADEQUATELY PROVIDE SAFETY FOR THEIR OPERATIONS DURING CONSTRUCTION, IN COMPLIANCE WITH FEDERAL, STATE, AND LOCAL LAWS INCLUDING SAFETY FOR POTENTIAL UNAUTHORIZED PEDESTRIAN USAGE. PEDESTRIAN SEPARATION SHALL BE IN ACCORDANCE WITH SCD MT-110.10. IT WILL BE THE CONTRACTORS RESPONSIBILITY TO ERECT, MAINTAIN, ADJUST, AND REMOVE THE SAFETY DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE BID FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN.

GENERAL

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN.

ADDITIONAL TRAFFIC CONTROL ITEMS MAY BE DEEMED NECESSARY BY THE ENGINEER TO ENSURE THE SAFETY OF THE TRAVELING PUBLIC AND SAFETY OF WORKERS WITHIN THE CONSTRUCTION ZONE. ANY ADDITIONAL WORK INCLUDING LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NOT SPECIFICALLY ITEMIZED BUT REQUIRED FOR MAINTAINING TRAFFIC AND SAFETY DURING CONSTRUCTION AS DIRECTED BY THE ENGINEER SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN.

ITEM 614- MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER AND/OR CALCIUM CHLORIDE FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 - WATER 5 M GAL

MAINTENANCE OF TRAFFIC (DRIVEWAY ACCESS)

IN ORDER TO MINIMIZE POTENTIAL CONFLICTS, THE CONTRACTOR SHALL CONTACT EACH PROPERTY OWNER OR TENANT THAT WILL BE AFFECTED BY THE PROPOSED DRIVEWAY APRON REPAIRS. A WRITTEN NOTICE SHOULD BE GIVEN TO EACH PROPERTY WITH A PHONE NUMBER FOR AN ONSITE REPRESENTATIVE OF THE CONTRACTOR AT LEAST 48 HOURS PRIOR TO ANY DRIVEWAY CLOSURE. THE NOTICE SHOULD STATE A TIME FRAME OF EXPECTED CLOSURE AND SUGGESTED LOCATIONS FOR RESIDENTS TO PARK WHICH WILL BE COORDINATED WITH THE ENGINEER. THE CONTRACTOR SHOULD MAKE THEIR BEST EFFORT TO ACCOMMODATE RESIDENTS OR OWNERS WHO REQUIRE ANY SPECIAL NEEDS.

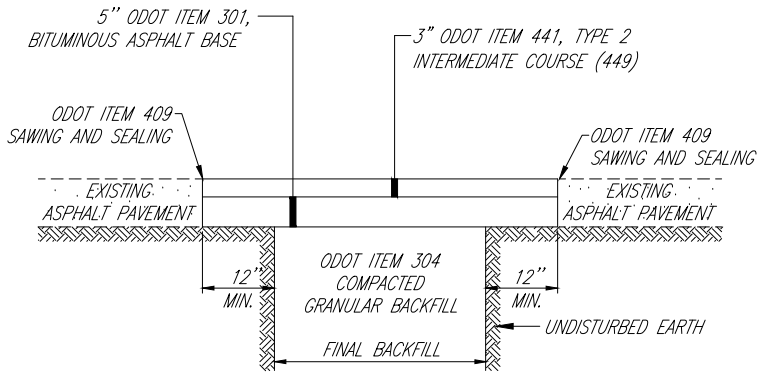
DRIVEWAYS ARE ONLY TO BE CLOSED DURING THE INSTALLATION AND CURING TIMES FOR CONCRETE ITEMS. AT ALL OTHER TIMES (EXCAVATION, STORM SEWER INSTALLATION, ETC.) DRIVES SHALL BE MAINTAINED WITH THE USE OF PLATES AND/OR GRANULAR MATERIAL, UNLESS THE WORK IS PERFORMED ON THE SAME DAY OF THE CONCRETE POUR (TEMPORARY ACCESS AND STEEL PLATE COSTS ARE TO BE INCLUDED IN THE BID PRICE FOR ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN).

DRIVES SHALL BE OPENED TO TRAFFIC AS FOLLOWS:

CURB: BEAM BREAK OF 600 PSI OR GREATER

DRIVE APRONS (CLASS MS CONCRETE): BEAM BREAK OF 600 PSI OR GREATER OR 24 HOURS (WHICHEVER IS LESS IN DURATION)

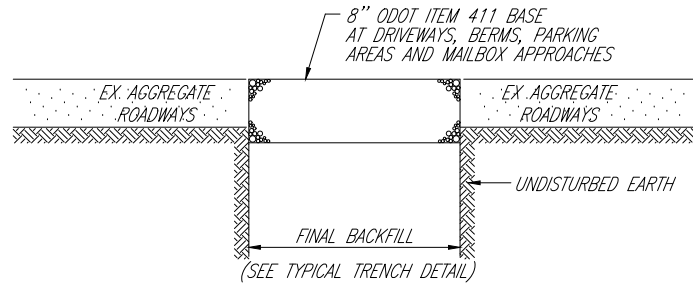
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- NOTES:
1. PLACE ODOT ITEM 407 TACK COAT BETWEEN ALL ASPHALT LAYERS AS PER CMS TABLE 407.06-1. QUANTITY TO BE INCLUDED WITH PERTINENT ASPHALT ITEM FOR PAYMENT. NO SEPARATE PAYMENT SHALL BE MADE.
 2. USE THIS ASPHALT MAKE-UP FOR ALL NON-RESIDENTIAL REPLACEMENTS.
 3. WHERE CALLED FOR ON THE PLAN, FINAL BACKFILL SHALL BE LOW STRENGTH MORTAR BACKFILL.
 4. MAX. PAYMENT LIMIT FOR ASPHALT REPLACEMENT IS TRENCH WIDTH + 24".
 5. ACCEPTANCE SHALL BE PER ODOT ITEM 449.

1 TYPE A ASPHALT PAVEMENT REPLACEMENT - STREETS

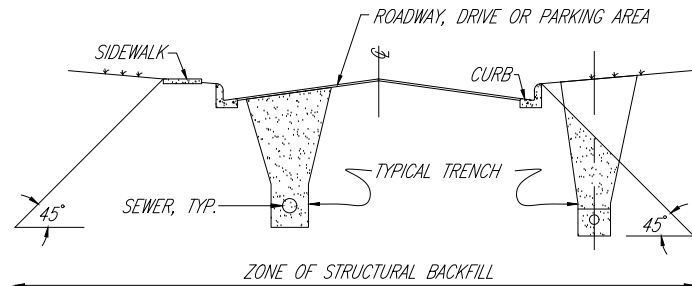
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- NOTES:
1. AGGREGATE MATERIAL TYPE SHALL MATCH EXISTING (GRAVEL, SLAG OR LIMESTONE).
 2. FINAL BACKFILL AT MAILBOX APPROACHES SHALL BE SUITABLE SOIL OR GRANULAR BACKFILL COMPACTED IN 8 INCH LAYERS TO A DENSITY NOT LESS THAN 96% PROCTOR.

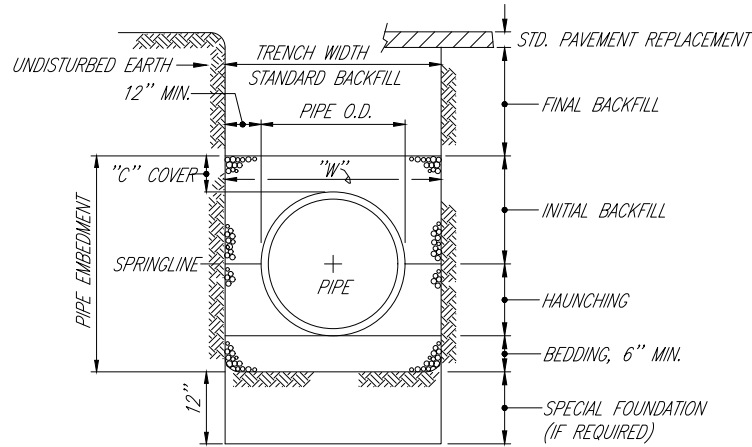
2 TYPE B GRANULAR PAVEMENT REPLACEMENT

SCALE: NTS



- NOTES:
1. ALL BACKFILL WITHIN THE DESCRIBED ZONE BELOW THE 45° LINE SHALL BE COMPACTED STRUCTURAL BACKFILL CONFORMING WITH ODOT ITEM 703.11.
 2. BACKFILL ABOVE THE 45° LINE SHALL BE SUITABLE SOIL OR GRANULAR MATERIAL.

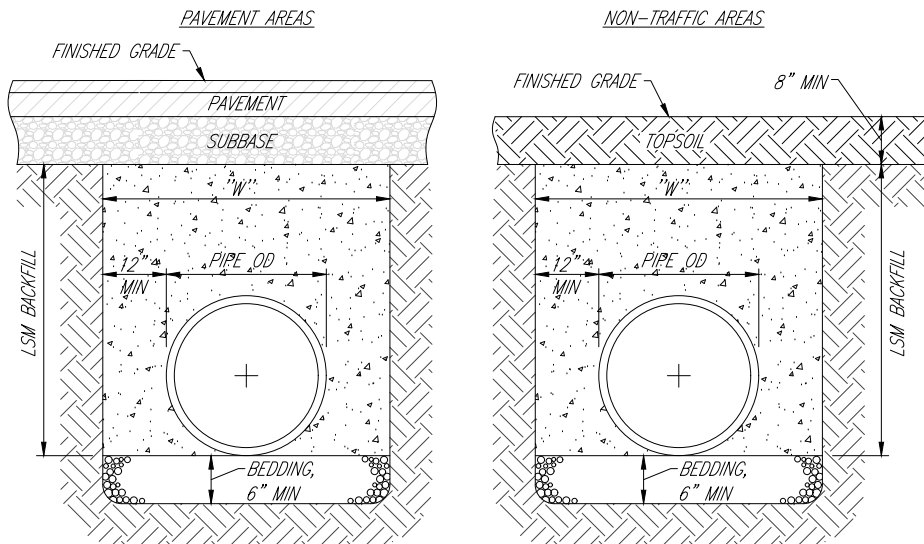
3 TRENCH BACKFILL REQUIREMENTS



- NOTES:
1. FOUNDATION SHALL BE FIRM, STABLE, UNIFORM SUPPORT. SPECIAL FOUNDATION, IF REQUESTED BY THE ENGINEER, SHALL BE COMPACTED BEDDING MATERIAL.
 2. WORK SHALL BE PERFORMED IN ACCORDANCE WITH ODOT ITEM 611 CONDUITS.
 3. PIPE EMBEDMENT SHALL BE ASTM #57 OR #67 CRUSHED GRADED LIMESTONE AGGREGATE AS DEFINED IN ASTM D2321, CLASS 1, UNLESS OTHERWISE SPECIFIED BY THE PIPE MANUFACTURER.
 4. ALL HAUNCHING AGGREGATE SHALL BE SHOVEL SLICED TO FILL VOIDS, AND COMPACTED IN PLACE.
 5. FINAL BACKFILL AT ALL ROADWAYS, DRIVES, WALKS, BERMS AND PARKING AREAS SHALL BE ODOT ITEM 703.11, TYPE 1, 304 COMPACTED IN 8" LAYERS, UNLESS OTHERWISE SPECIFIED BY THE PIPE MANUFACTURER.
 6. MINIMUM TRENCH WIDTH (W) AT THE TOP OF ALL FLEXIBLE CONDUITS SHALL BE PER ASTM D2321.
 7. MAXIMUM TRENCH WIDTH (W) AT THE TOP OF ALL RIGID CONDUITS SHALL BE PIPE O.D. + 24 INCHES.
 8. MAXIMUM TRENCH WIDTH (W) FOR PAYMENT MEASUREMENT IS PIPE O.D. + 24 INCHES.
 9. "C" MINIMUM = 6" RIGID CONDUIT, 12" FLEXIBLE CONDUITS.

4 TYPICAL TRENCH FOR SANITARY SEWERS, STORM SEWERS AND FORCE MAINS

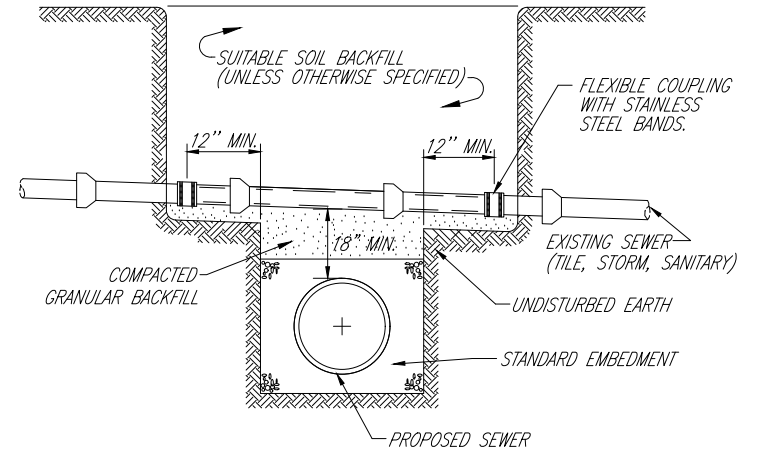
SCALE: NTS



- NOTES:
1. WORK SHALL BE PERFORMED IN ACCORDANCE WITH ODOT 611 AND 499.
 2. MINIMUM TRENCH WIDTH ("W") SHALL BE PIPE OD + 24".
 3. FOUNDATION SHALL BE FIRM, STABLE, UNIFORM SUPPORT.
 4. PIPE BEDDING SHALL BE #57 OR #67.
 5. ANCHOR PIPE OR PLACE LSM IN LIFTS TO PREVENT FLOATING.
 6. BACKFILL SHALL BE ODOT 613 LOW STRENGTH MORTAR BACKFILL. FILL ENTIRE TRENCH WIDTH AND PLACE LSM UP TO THE BOTTOM OF SUBBASE.

5 LOW STRENGTH MORTAR BACKFILL

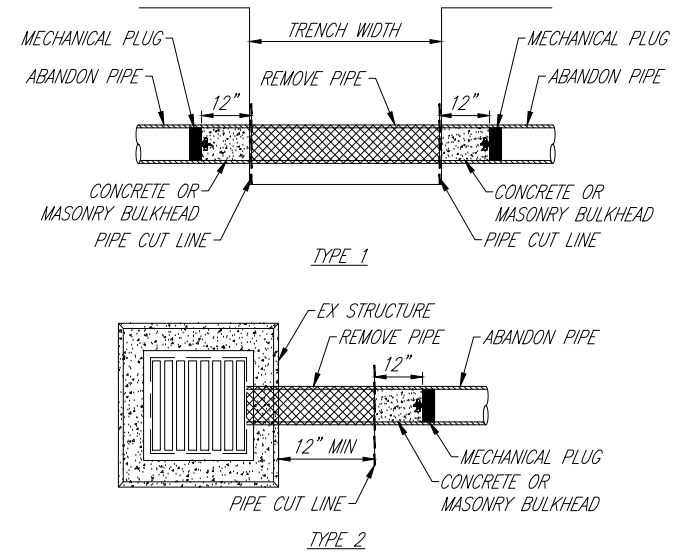
SCALE: NTS



- NOTES:
1. ALL CONDUITS REPLACED UNDER ROADWAYS, BERMS, DRIVES AND PARKING AREAS SHALL BE EMBEDDED AND BACKFILLED AS CALLED FOR ON THE PLANS.
 2. ALL STORM CONDUITS AND FIELD TILE SHALL BE REPLACED WITH IN KIND OR BETTER MATERIAL.
 3. SANITARY SEWER SHALL BE ODOT 707.45, ASTM D3035, SDR 35.
 4. ALL COSTS FOR UTILITY CROSSING REPLACEMENT TO BE INCLUDED WITH PERTINENT 611 CONDUIT ITEM.
 5. IF 18" MIN. CLEARANCE CANNOT BE ACHIEVED THEN LOW STRENGTH MORTAR BACKFILL SHALL BE USED.

6 TYPICAL UTILITY CROSSING REPLACEMENT

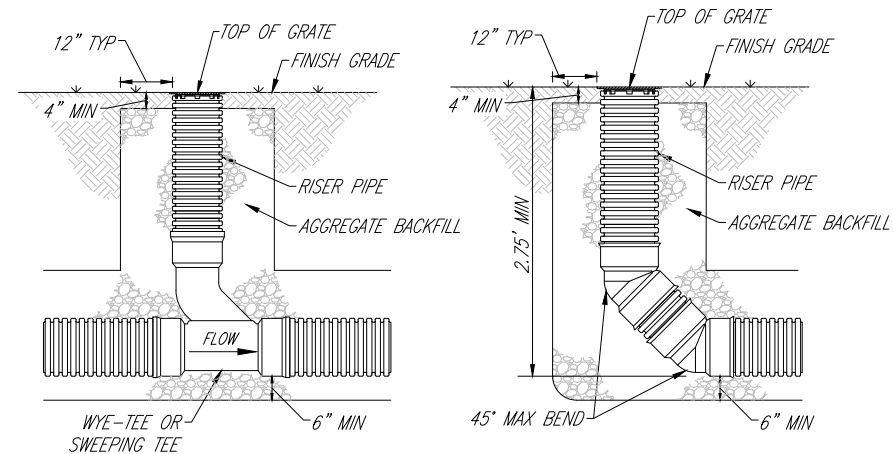
SCALE: NTS



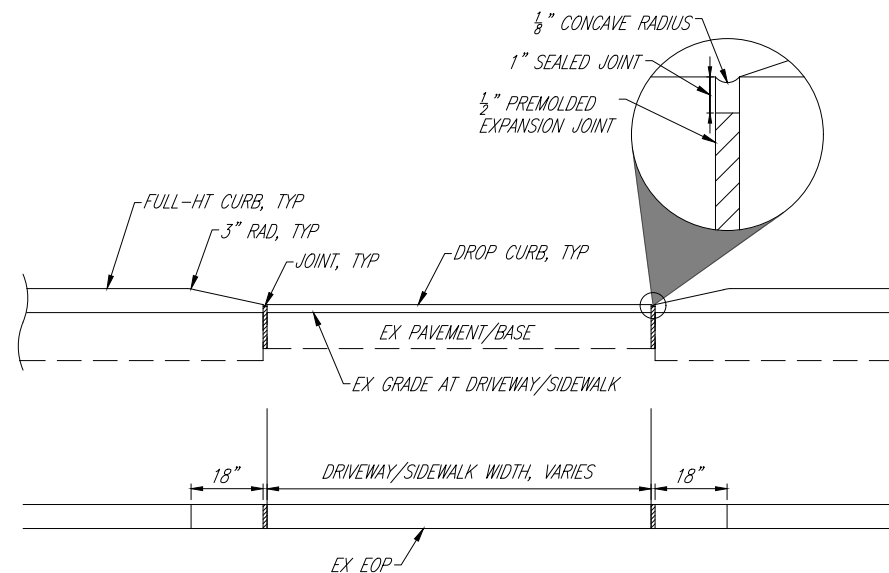
- NOTES:
1. CONCRETE BULKHEAD SHALL BE 4000 PSI @ 28 DAYS.
 2. MASONRY BULKHEAD SHALL CONTAIN NON-SHRINK GROUT AS PER CMS 705.20.

7 PIPE ABANDONMENT

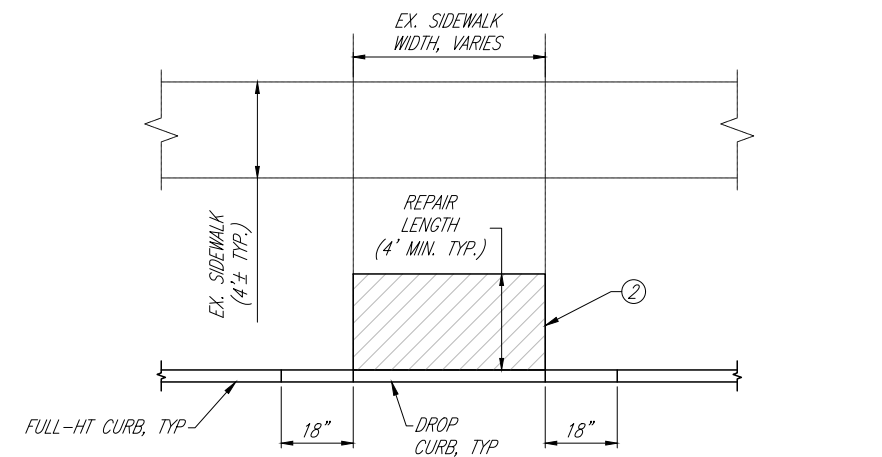
SCALE: NTS



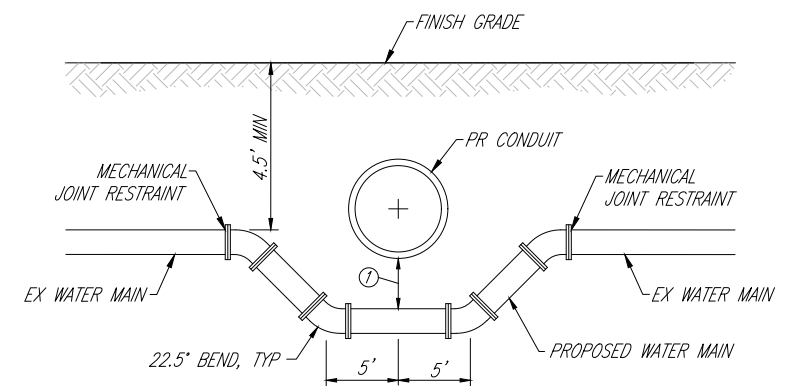
- NOTES:
1. REFER TO PLAN AND PROFILE SHEETS FOR STRUCTURE SIZES AND LOCATIONS.
 2. GRATE SHALL BE PEDESTRIAN SAFE AND FLUSH WITH FINISH GRADE.
 3. AGGREGATE BEDDING AND BACKFILL SHALL BE ODOT 703.11.
 4. ALL FITTINGS SHALL HAVE A MAX 45° BEND.
 5. REFER TO TYPICAL TRENCH DETAIL FOR PIPE EMBEDMENT AND BACKFILL REQUIREMENTS.



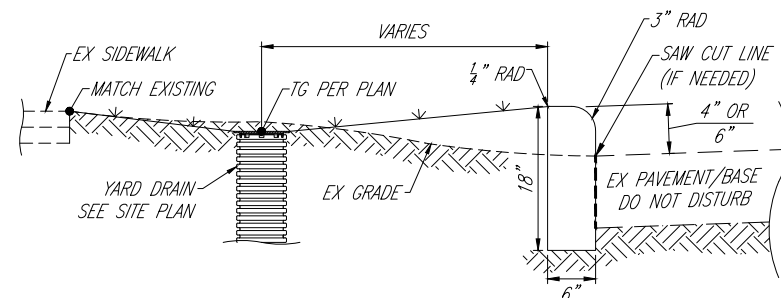
- NOTES:
1. EXPANSION JOINT IS NOT REQUIRED FOR CURB ADJACENT TO FLEXIBLE PAVEMENT.
 2. STANDARD 2" HIGH DROP CURB SHALL BE USED AS PER ODOT SCD BP-4.1
 3. REFER TO TYPICAL CURB SECTION FOR ADDITIONAL INFORMATION.
 4. REFER TO PLAN AND PROFILE SHEETS FOR DROP CURB LOCATIONS.



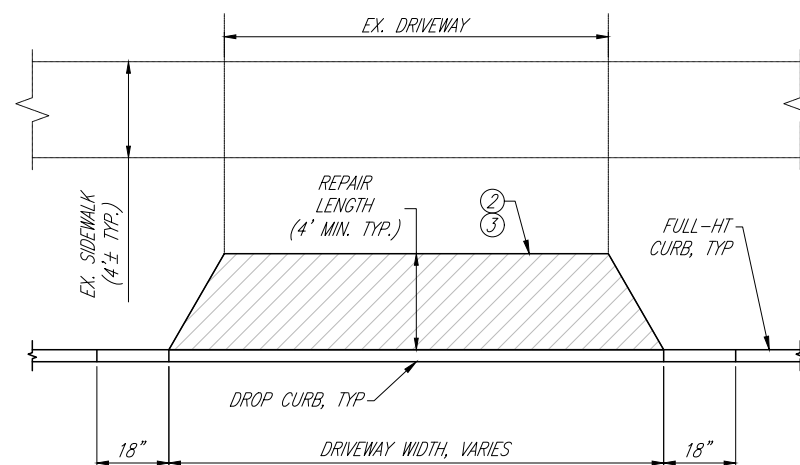
- NOTES:
1. SEE DRIVEWAY/SIDEWALK DROP CURB DETAIL FOR CURB HEIGHT TRANSITIONS.
 2. EX. CONCRETE SIDEWALKS SHALL BE REPAIRED FULL DEPTH WITH ITEM 608 - 4" CONCRETE WALK.
 3. REFER TO TYPICAL CURB SECTION FOR ADDITIONAL INFORMATION.



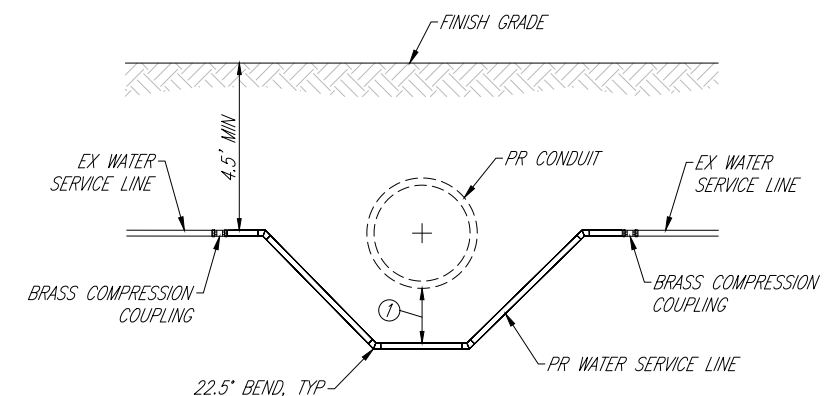
- NOTES:
1. 18" MIN CLEARANCE FOR SANITARY AND STORM SEWER.
 2. REFER TO CONCRETE THRUST BLOCK DETAIL FOR BLOCKING ON BENDS.
 3. ALL PIPE AND FITTINGS ARE TO BE WRAPPED WITH POLYETHYLENE WRAP.
 4. PIPE MATERIAL SHALL MEET ODOT ITEM 638.02 REQUIREMENTS.



- NOTES:
1. REFER TO ODOT SCD BP-5.1 FOR ADDITIONAL DETAILS.
 2. REFER TO PLAN AND PROFILE SHEETS FOR CURB LOCATION.
 3. SAW CUT A SMOOTH FINISHED SURFACE, IF NEEDED, PER ODOT 409.
 4. SEAL JOINT WITH MATERIAL PER ODOT 705.04.
 5. IF PAVEMENT OR BASE IS DAMAGED DURING CURB INSTALLATION, IT SHALL BE REPLACED AT THE DIRECTION OF THE ENGINEER. REFER TYPICAL PAVEMENT REPAIR DETAIL.
 6. FILL MATERIAL BEHIND CURB SHALL BE TOPSOIL PER ODOT 652 OR 653.
 7. GRADE BEHIND CURB TO PROVIDE ADEQUATE DRAINAGE TO DRAIN BASINS.
 8. ALL DISTURBED GROUND SHALL BE SEEDED AND MULCHED PER ODOT 659, CLASS 1.
 9. ALL ITEMS INCLUDED IN THIS DETAIL (EXCEPT THE YARD DRAIN) ARE INCIDENTAL TO EITHER ITEM 609, CURB TYPE 6, AS PER PLAN OR ITEM 609, CURB TYPE 4-C, AS PER PLAN.



- NOTES:
1. SEE DRIVEWAY/SIDEWALK DROP CURB DETAIL FOR CURB HEIGHT TRANSITIONS.
 2. EX. CONCRETE & ASPHALT DRIVEWAYS SHALL BE REPAIRED FULL DEPTH WITH ITEM 452 - 6" NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN.
 3. EX. GRAVEL DRIVEWAYS SHALL BE REPAIRED WITH ITEM 411 - STABILIZED CRUSHED AGGREGATE, AS PER PLAN.
 4. REFER TO TYPICAL CURB SECTION FOR ADDITIONAL INFORMATION.



- NOTES:
1. 18" MIN CLEARANCE FOR SANITARY AND STORM SEWER.
 2. PIPE MATERIAL SHALL MEET ODOT ITEM 638.02 REQUIREMENTS.

DESIGNED

TGW

REVIEWED

JDB

KEM

K.E. MCARTNEY & ASSOCIATES

ENGINEERS-PLANNERS-SURVEYORS

GENERAL SUMMARY

VILLAGE OF PLYMOUTH

DRAINAGE IMPROVEMENTS

9

16

SHEET NUMBER

ITEM

ITEM
EXT.

GRAND
TOTAL

UNIT

DESCRIPTION

SEE SHEET
NO.

3-4

5

9

10-14

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DRAINAGE SUBSUMMARY						202	202	202	202	611	611	611	611	611	611	611	611	611	611	611	611	611
SHEET NO.	STRUCTURE NO.					PIPE REMOVED, 24" AND UNDER	MANHOLE REMOVED	CATCH BASIN REMOVED	REMOVAL MISC.: PLUG AND ABANDON EXISTING CONDUIT	8" CONDUIT, TYPE B, AS PER PLAN	8" CONDUIT, TYPE C, AS PER PLAN	12" CONDUIT, TYPE B, AS PER PLAN	12" CONDUIT, TYPE C, AS PER PLAN	15" CONDUIT, TYPE B, AS PER PLAN	18" CONDUIT, TYPE B, AS PER PLAN	24" CONDUIT, TYPE B, AS PER PLAN	CATCH BASIN, NO. 3, AS PER PLAN	CATCH BASIN, NO. 3A, AS PER PLAN	CATCH BASIN, NO. 6, AS PER PLAN	INLET, SIDE DITCH, AS PER PLAN	MANHOLE, NO. 3, AS PER PLAN	MANHOLE ADJUSTED TO GRADE, AS PER PLAN
		FROM	TO	FROM	TO																	
		STATION	OFFSET	STATION	OFFSET	FT	EACH	EACH	EACH	FT	FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	EACH	EACH	EACH
W BROADWAY STREET																						
10	S-1	11+27.40	9.64 LT																			1
11	S-2	14+95.52	9.39 LT																			1
11	CB-1	15+50.12	16.00 RT	15+50.12	14.00 LT	34		1		5		30					1					
11	CB-2	15+50.12	14.00 LT	19+05.34	14.01 LT	43	1	1	2	5	11	355					1					
11	CB-3	18+05.33	14.25 RT	19+05.33	14.25 RT							100					1					
12	S-3	18+98.03	9.39 LT																			1
12	D-1	19+05.38	19.22 RT							5		5							1			
12	CB-4	19+05.33	14.25 RT	19+05.34	14.01 LT	39		1						28		1						
12	CB-5	19+05.34	14.01 LT	19+05.81	33.67 LT	21									21			1				
12	MH-1	19+05.81	33.67 LT			15						10			5						1	
12	D-2	19+38.95	19.17 RT									5							1			
12	D-3	19+56.70	20.37 LT									5							1			
12	D-4	19+80.50	20.35 LT			10			1			5							1			
12	CB-6	19+05.34	14.01 LT	19+80.51	14.20 LT	3		1	1					75		1						
12	CB-7	19+05.33	14.25 RT	19+80.51	14.25 RT	79		1	1			75				1						
12	CB-8	19+80.51	14.25 RT	20+55.64	15.63 RT	85						85					1					
12	D-5	20+66.97	20.17 LT									5							1			
12	D-6	21+99.80	19.58 LT									5							1			
12	S-4	22+80.57	4.15 LT																			1
12	D-7	22+92.02	20.01 LT			10			1			5							1			
12	CB-9	19+80.51	14.20 LT	22+92.07	14.17 LT			1						312			1					
13	D-8	23+30.58	20.07 LT									5							1			
13	D-9	23+81.87	19.89 LT									5							1			
13	CB-10	22+92.07	14.17 LT	24+53.21	13.75 LT	3			2			161					1					
13	CB-11	24+53.21	15.90 LT	24+53.21	15.90 RT	3		1	1			30					1					
13	S-5	26+54.81	7.36 LT																			1
13	CB-12	24+53.21	13.75 LT	26+63.88	14.00 LT	4		1	1			211					1					
14	CB-13	26+63.88	14.00 LT	29+45.74	14.69 LT							282					1					
14	CB-14	29+45.74	14.69 LT	29+45.91	15.86 RT	3		1	1			31					1					
14	S-6	30+42.50	9.23 LT																			1
W BROADWAY STREET TOTALS						352	1	9	11	10	16	1360	55	312	103	26	3	10	1	9	1	6
TOTALS CARRIED TO GENERAL SUMMARY						352	1	9	11	10	16	1360	55	312	103	26	3	10	1	9	1	6

<u>PAVEMENT MARKING SUBSUMMARY</u>					642	642
SHEET NO.	LOCATION	STATION		SIDE	CENTER LINE, TYPE 1	CROSSWALK LINE, 12", TYPE 1
		FROM	TO		MILE	FT
<i>W BROADWAY STREET</i>						
10-14	CENTERLINE CONST.	10+35.00	30+75.50	CL	0.4	
13	PARK AVENUE	26+27.58	26+50.84	LT		44.9
13	ALLEY	27+06.24	27+18.23	RT		24.0
14	BELL STREET	28+99.79	29+29.44	RT		52.2
14	ALLEY	29+69.80	29+86.01	LT		28.6
<i>W BROADWAY STREET TOTALS</i>					0.4	150.0
<i>TOTALS CARRIED TO GENERAL SUMMARY</i>					0.40	150

DRIVEWAY SUBSUMMARY							204	411	452	608
SHEET NO.	REFERENCE NO.	BEGINNING STATION	ENDING STATION	SIDE	EX. APRON TYPE	DRIVEWAY/SIDEWALK WIDTH (FT)	SUBGRADE COMPACTION, AS PER PLAN	STABILIZED CRUSHED AGGREGATE, AS PER PLAN	6" NON-REINFORCED CONCRETE PAVEMENT, CLASS QC MS	4" CONCRETE WALK
							SY	CY	SY	SF
W BROADWAY STREET										
10	DR-1	11+39.48	11+56.01	RT	CONC	16.5	7.3		7.3	
10	DR-2	11+84.72	12+03.90	LT	CONC	19.2	8.5		8.5	
10	DR-3	12+45.92	12+63.64	RT	ASPH	17.7	7.9		7.9	
10	DR-4	12+54.54	12+71.48	LT	ASPH	16.9	7.5		7.5	
10	DR-5	13+10.94	13+23.81	RT	ASPH	12.9	5.7		5.7	
10	DR-6	13+45.58	13+58.52	LT	ASPH	12.9	5.7		5.7	
11	DR-7	14+20.23	14+37.56	LT	ASPH	17.3	7.7		7.7	
11	DR-8	14+29.97	14+42.39	RT	ASPH	12.4	5.5		5.5	
11	DR-9	14+50.02	14+74.48	LT	CONC	24.5	10.9		10.9	
11	SW-1	14+90.43	14+95.81	LT	CONC	5.4	2.4			21.6
11	DR-10	14+92.93	15+19.09	RT	CONC	26.2	11.6		11.6	
11	DR-11	15+81.73	15+97.46	RT	GRAV	15.7	7.0	1.6		
11	DR-12	16+52.37	16+64.70	RT	GRAV	12.3	5.5	1.2		
11	DR-13	16+79.26	17+13.45	LT	GRAV	34.2	15.2	3.4		
11	DR-14	17+02.67	17+12.85	RT	CONC	10.2	4.5		4.5	
11	DR-15	17+31.77	17+61.86	LT	GRAV	30.1	13.4	3.0		
12	DR-16	18+89.03	19+13.52	LT	GRAV	24.5	10.9	6.3		
12	DR-17	19+21.32	19+36.31	RT	GRAV	15.0	6.7	1.5		
12	SW-2	19+61.72	19+65.44	LT	CONC	3.7	1.6			14.8
12	DR-18	20+13.30	20+23.62	RT	CONC	10.3	4.6		4.6	
12	DR-19	20+31.35	20+44.46	RT	CONC	13.1	5.8		5.8	
12	DR-20	20+41.47	20+65.86	LT	ASPH	24.4	10.8		10.8	
12	SW-3	20+75.83	20+78.62	RT	CONC	2.8	1.2			11.2
12	DR-21	21+30.69	21+40.21	LT	GRAV	9.5	4.2	0.9		
12	DR-22	21+79.31	22+04.97	RT	ASPH	25.7	11.4		11.4	
12	DR-23	21+82.69	21+95.87	LT	GRAV	13.2	5.9	1.3		
12	DR-24	22+64.40	22+84.68	RT	CONC	20.3	9.0		9.0	
13	SW-4	23+21.55	23+25.55	LT	CONC	4.0	1.8			16.0
13	DR-25	23+61.93	23+81.17	LT	ASPH	19.2	8.5		8.5	
13	DR-26	24+69.90	24+78.82	LT	GRAV	8.9	4.0	0.9		
13	DR-27	25+03.73	25+19.13	LT	GRAV	15.4	6.8	1.5		
13	SW-5	25+40.32	25+44.32	RT	CONC	4.0	1.8			16.0
13	DR-28	25+61.10	25+70.00	LT	ASPH	8.9	4.0		4.0	
13	SW-6	26+15.56	26+18.42	RT	CONC	2.9	1.3			11.6
13	SW-7	26+65.63	26+69.63	LT	CONC	4.0	1.8			16.0
13	SW-8	26+67.79	26+71.79	RT	CONC	4.0	1.8			16.0
13	SW-9	27+21.70	27+24.27	LT	CONC	2.6	1.2			10.4
14	DR-29	28+05.72	28+18.23	RT	ASPH	12.5	5.6		5.6	
14	SW-10	28+17.97	28+21.97	LT	CONC	4.0	1.8			16.0
14	SW-11	28+69.78	28+72.59	LT	CONC	2.8	1.2			11.2
14	SW-12	30+52.34	30+56.34	LT	CONC	4.0	1.8			16.0
14	SW-13	30+56.24	30+58.74	RT	CONC	2.5	1.1			10.0
W BROADWAY STREET TOTALS							242.9	21.6	142.7	186.8
TOTALS CARRIED TO GENERAL SUMMARY							243	22	143	187

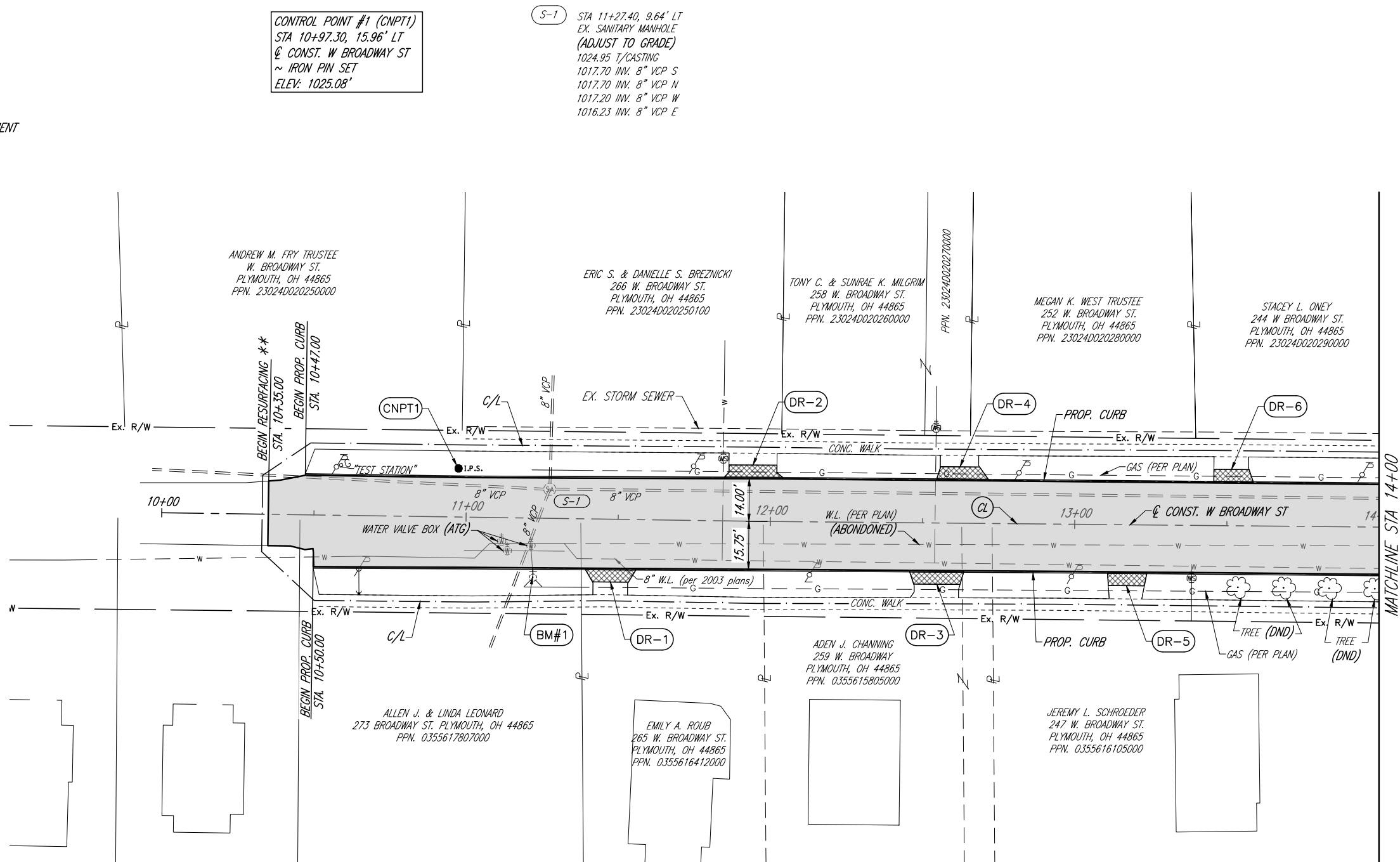
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LEGEND

- ITEM 411 - STABILIZED CRUSHED AGGREGATE
- ITEM 441 - ASPHALT CONCRETE RESURFACING
- ITEM 452 - 6" NON-REINFORCED CONCRETE PAVEMENT
- ITEM 608 - 4" CONCRETE WALK
- ASPHALT PAVEMENT REPLACEMENT.
SEE SHEET Z FOR DETAILS
- (G) POSSIBLE GAS LINE CONFLICT
- (S) ITEM 638 - SPECIAL - LOWER
WATER SERVICE CONNECTION
- (W) ITEM 638 - WATER WORKS, MISC.:
LOWERING WATER LINE
- (CL) ITEM 642 - CENTERLINE, TYPE 1
- (CW) ITEM 642 - CROSSWALK LINE, 12", TYPE 1
- (CC) CONCRETE COLLAR AS PER ODOT
SCD DM-1.1
- (ATG) ADJUST TO GRADE
- (DND) DO NOT DISTURB

UTILITY NOTES:

1. ALL UTILITIES LABELED "PER PLAN" WERE NOT MARKED
IN THE FIELD AND WILL NEED TO BE VERIFIED BY THE
OWNER.

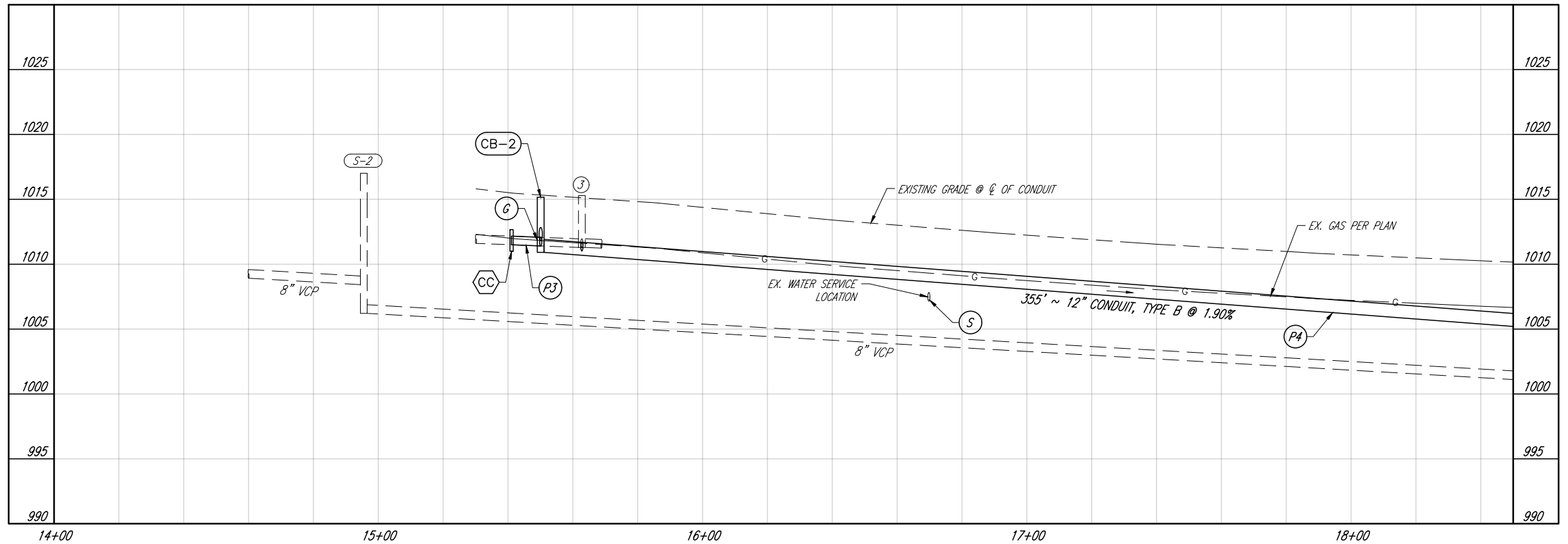
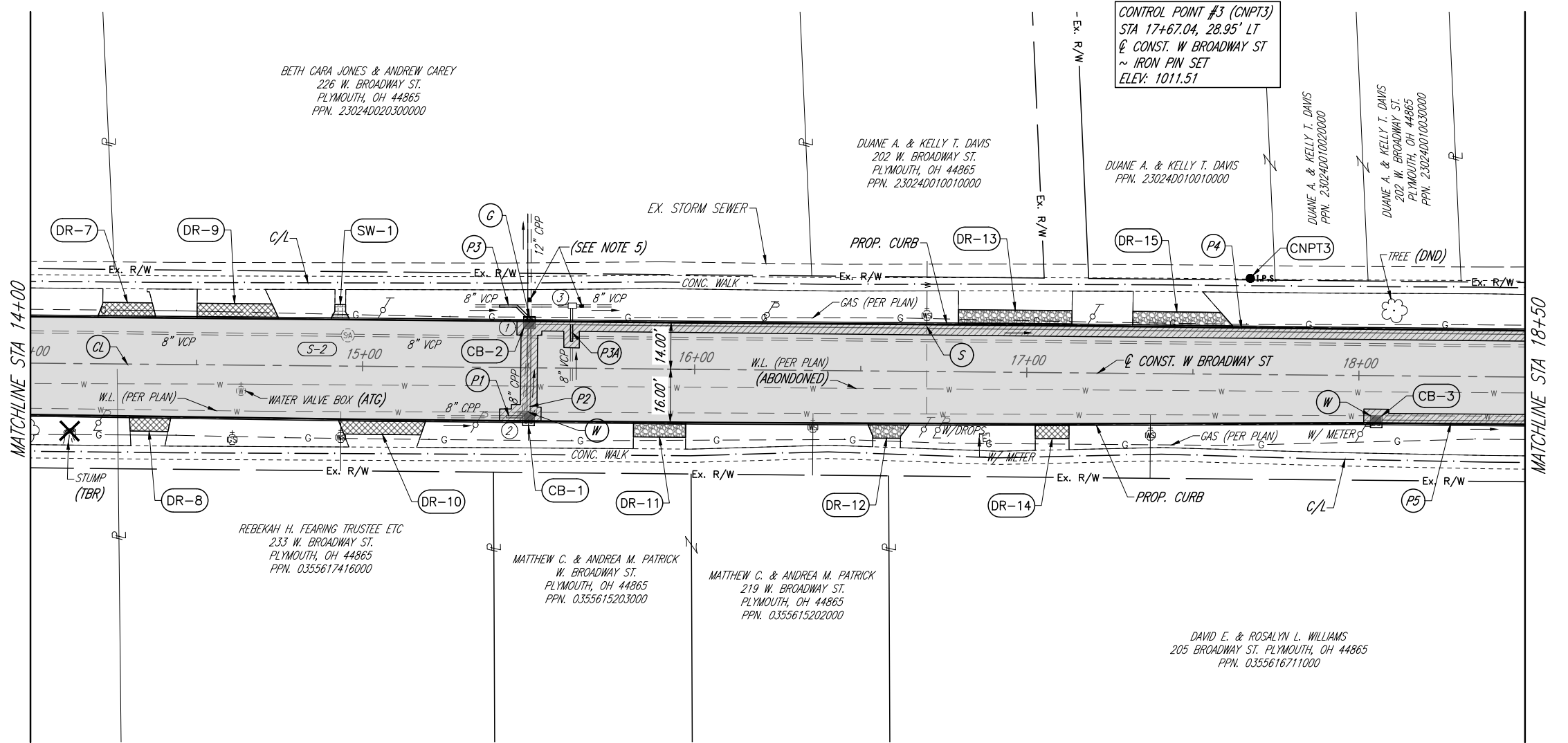


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- ① STA 15+50.12, 13.41' LT
EX. CATCH BASIN
(TO BE REMOVED)
1015.08 T/GRATE
1012.28 INV. 8" VCP S
1012.28 INV. 12" CPP N
- ② STA 15+48.63, 14.75' RT
EX. CATCH BASIN
(TO BE REMOVED)
1015.24 T/GRATE
1012.54 INV. 8" CPP W
1012.49 INV. 8" CPP N
- ③ STA 15+63.01, 18.82' LT
EX. STORM MANHOLE
(TO BE REMOVED)
1015.29 T/CASTING
1011.29 INV. 8" VCP W
1011.29 INV. 8" VCP S
1011.29 INV. 8" VCP E
- ④ STA 15+50.12, 16.00' RT
PR CB, ODOT No. 3A
1015.12 T/GRATE
1012.54 INV 8" (W)
1012.12 INV 12" (N)
- ⑤ STA 15+50.12, 14.00' LT
PR CB, ODOT No. 3A
1015.16 T/GRATE
1011.83 INV 12" (S)
1011.41 INV 8" (N)
1010.91 INV 12" (E)
- ⑥ STA 14+95.52, 9.39' LT
EX. SANITARY MANHOLE
(ADJUST TO GRADE)
1017.01 T/CASTING
1008.56 INV. 8" VCP W
1006.21 INV. 8" VCP E
- ⑦ 5' ~ 8" CONDUIT, TYPE B,
@ EX. SLOPE
(SEE NOTE 4)
- ⑧ 11' ~ 8" CONDUIT, TYPE C,
@ EX. SLOPE
(SEE NOTES 2 & 4)
- ⑨ 5' ~ 8" CONDUIT, TYPE B,
@ EX. SLOPE
(SEE NOTE 4)
- ⑩ 355' ~ 12" CONDUIT, TYPE B,
@ 1.90% SLOPE
- ⑪ STA 18+05.33, 14.25' RT
PR CB, ODOT No. 3A
1010.53 T/GRATE
1007.53 INV 12" (E)
- ⑫ 100' ~ 12" CONDUIT, TYPE B,
@ 2.23% SLOPE
(SEE NOTE 3)

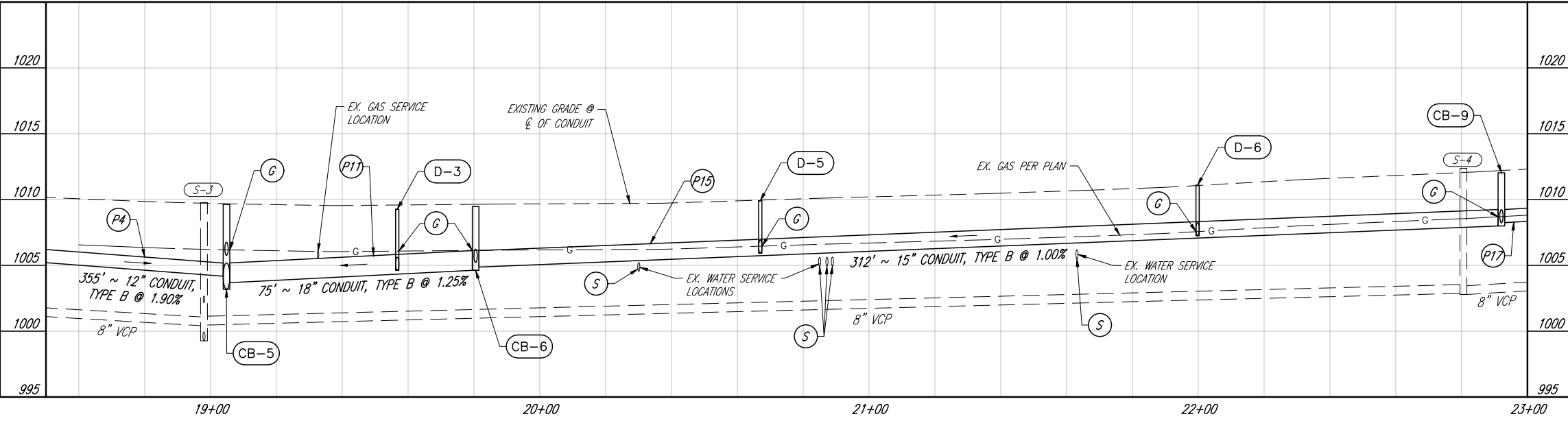
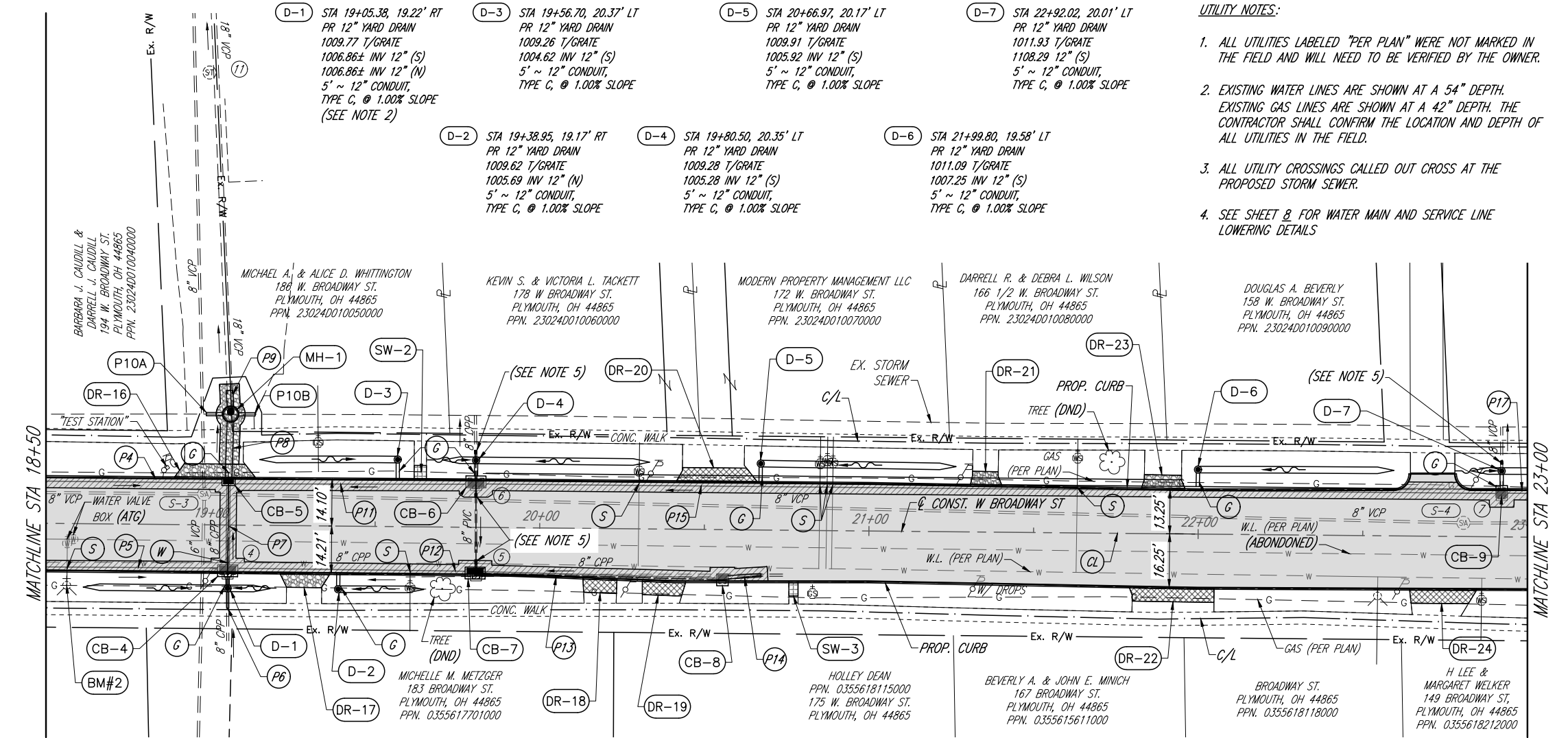
- NOTES:
- SEE SHEET 11 FOR ITEM LEGEND.
 - SEE SHEET 16 FOR STORM SEWER LATERAL CONNECTION PROFILES.
 - SEE SHEET 16 FOR STORM SEWER PROFILES ON THE SOUTH EDGE OF WEST BROADWAY STREET.
 - MAKE CONNECTION TO EXISTING CONDUIT WITH CONCRETE COLLAR AS PER ODOT SCD DM-1.1.
 - EXISTING PIPE TO BE ABANDONED IN PLACE AND PLUGGED IN ACCORDANCE WITH DETAIL Z ON SHEET Z.

- UTILITY NOTES:
- ALL UTILITIES LABELED "PER PLAN" WERE NOT MARKED IN THE FIELD AND WILL NEED TO BE VERIFIED BY THE OWNER.
 - EXISTING WATER LINES ARE SHOWN AT A 54" DEPTH. EXISTING GAS LINES ARE SHOWN AT A 42" DEPTH. THE CONTRACTOR SHALL CONFIRM THE LOCATION AND DEPTH OF ALL UTILITIES IN THE FIELD.
 - ALL UTILITY CONFLICTS CALLED OUT CROSS AT THE PROPOSED STORM SEWER.
 - SEE SHEET 8 FOR WATER MAIN AND SERVICE LINE LOWERING DETAILS



- ④ STA 19+05.33, 14.73' RT
EX. CATCH BASIN
(TO BE REMOVED)
1009.53 T/GRATE
1006.83 INV. 8" CPP S
1006.63 INV. 8" CPP N
- ⑤ STA 19+80.78, 14.59' RT
EX. CATCH BASIN
(TO BE REMOVED)
1009.52 T/GRATE
1006.27 INV. 8" CPP E
1006.35 INV. 8" CPP W
- ⑥ STA 19+80.53, 12.83' LT
EX. CATCH BASIN
(TO BE REMOVED)
1009.55 T/GRATE
1005.90 INV. 8" CPP N
1005.90 INV. 8" PVC S
- ⑦ STA 22+92.09, 12.69' LT
EX. CATCH BASIN
(TO BE REMOVED)
1011.97 T/GRATE
1008.12 INV. 8" VCP N
- ⑪ STA 18+98.46, 137.41' LT
EX. MANHOLE
(TO REMAIN)
1004.16 T/RIM
999.76 INV. 18" VCP N
999.66 INV. 18" VCP S
- P4 355' ~ 12" CONDUIT,
TYPE B, @ 1.90% SLOPE
- P5 100' ~ 12" CONDUIT,
TYPE B, @ 2.23% SLOPE
(SEE NOTE 3)
- CB-4 STA 19+05.33, 14.25' RT
PR CB, ODOT No. 3
1009.55 T/GRATE
1006.81 INV 12" (S)
1005.30 INV 12" (W)
1005.30 INV 12" (E)
1004.80 INV 18" (N)
- P6 5' ~ 8" CONDUIT,
TYPE C, @ EX. SLOPE
(SEE NOTES 2 & 6)
- P7 28' ~ 18" CONDUIT,
TYPE B, @ 1.93% SLOPE
(SEE NOTE 2)
- CB-5 STA 19+05.34, 14.01 LT
PR CB, ODOT No. 6
1009.63 T/GRATE
1004.26 INV 18" (S)
1004.18 INV 12" (W)
1003.68 INV 18" (E)
1003.18 INV 24" (N)
- P8 21' ~ 24" CONDUIT,
TYPE B, @ 1.95% SLOPE
(SEE NOTES 2 & 4)
- MH-1 STA 19+05.81, 33.67' LT
(SEE NOTE 4)
PR MH, ODOT No. 3
1009.75 T/RIM
1002.77 INV 24" (S)
1001.74 INV 24" (N)
- P9 5' ~ 24" CONDUIT,
TYPE B, @ EX. SLOPE
(SEE NOTES 2, 4 & 6)
- P10A 5' ~ 12"± CONDUIT,
TYPE C, @ EX. SLOPE
(SEE NOTES 4, 6 & 7)
- S-3 STA 18+98.03, 9.39' LT
EX. SANITARY MANHOLE
(ADJUST TO GRADE)
1009.77 T/CASTING
1002.17 INV. 6" VCP S
1000.42 INV. 8" VCP W
1000.47 INV. 8" VCP E
999.27 INV. 8" VCP N
- S-4 STA 22+80.57, 4.15' LT
EX. SANITARY MANHOLE
(ADJUST TO GRADE)
1012.37 T/CASTING
1002.79 INV. 8" VCP E
1002.87 INV. 8" VCP W
- B.M. #2
STA 18+56.49, 18.78' RT
EX. CONST. W BROADWAY ST
~ CHISELED "X" ON FRONT
FLANGE BOLT OF FIRE HYDRANT
ELEV: 1012.60'
- P10B 5' ~ 12"± CONDUIT,
TYPE C, @ EX. SLOPE
(SEE NOTES 4, 6 & 7)
- CB-6 STA 19+80.51, 14.20' LT
PR CB, ODOT No. 3
1009.47 T/GRATE
1005.23 INV 12" (N)
1004.87 INV 15" (E)
1004.62 INV 18" (W)
- P11 75' ~ 18" CONDUIT,
TYPE B, @ 1.25% SLOPE
- CB-7 STA 19+80.51, 14.25' RT
PR CB, ODOT No. 3
1009.37 T/GRATE
1006.05 INV 12" (E)
1006.05 INV 12" (W)
- P12 75' ~ 12" CONDUIT,
TYPE B, @ 1.00% SLOPE
(SEE NOTE 3)
- P13 75' ~ 12" CONDUIT,
TYPE B, @ 1.00% SLOPE
(SEE NOTE 3)
- CB-8 STA 20+55.64, 15.63' RT
PR CB, ODOT No. 3A
1009.80 T/GRATE
1006.80± INV 12" (E)
1006.80 INV 12" (W)
- P14 10' ~ 12" CONDUIT,
TYPE B, @ EX. SLOPE
(SEE NOTES 3 & 6)
- P15 312' ~ 15" CONDUIT,
TYPE B, @ 1.00% SLOPE
- CB-9 STA 22+92.07, 14.17' LT
PR CB, ODOT No. 3A
1012.03 T/GRATE
1008.24 INV 12" (N)
1008.24 INV 12" (E)
1007.99 INV 15" (W)
- P16 NOT USED
- P17 161' ~ 12" CONDUIT,
TYPE B, @ 1.49% SLOPE

- D-1 STA 19+05.38, 19.22' RT
PR 12" YARD DRAIN
1009.77 T/GRATE
1006.86± INV 12" (S)
1006.86± INV 12" (N)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE
(SEE NOTE 2)
- D-2 STA 19+38.95, 19.17' RT
PR 12" YARD DRAIN
1009.62 T/GRATE
1005.69 INV 12" (N)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE
- D-3 STA 19+56.70, 20.37' LT
PR 12" YARD DRAIN
1009.26 T/GRATE
1004.62 INV 12" (S)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE
- D-4 STA 19+80.50, 20.35' LT
PR 12" YARD DRAIN
1009.28 T/GRATE
1005.28 INV 12" (S)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE
- D-5 STA 20+66.97, 20.17' LT
PR 12" YARD DRAIN
1009.91 T/GRATE
1005.92 INV 12" (S)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE
- D-6 STA 21+99.80, 19.58' LT
PR 12" YARD DRAIN
1011.09 T/GRATE
1007.25 INV 12" (S)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE
- D-7 STA 22+92.02, 20.01' LT
PR 12" YARD DRAIN
1011.93 T/GRATE
1108.29 INV 12" (S)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE



NOTES:

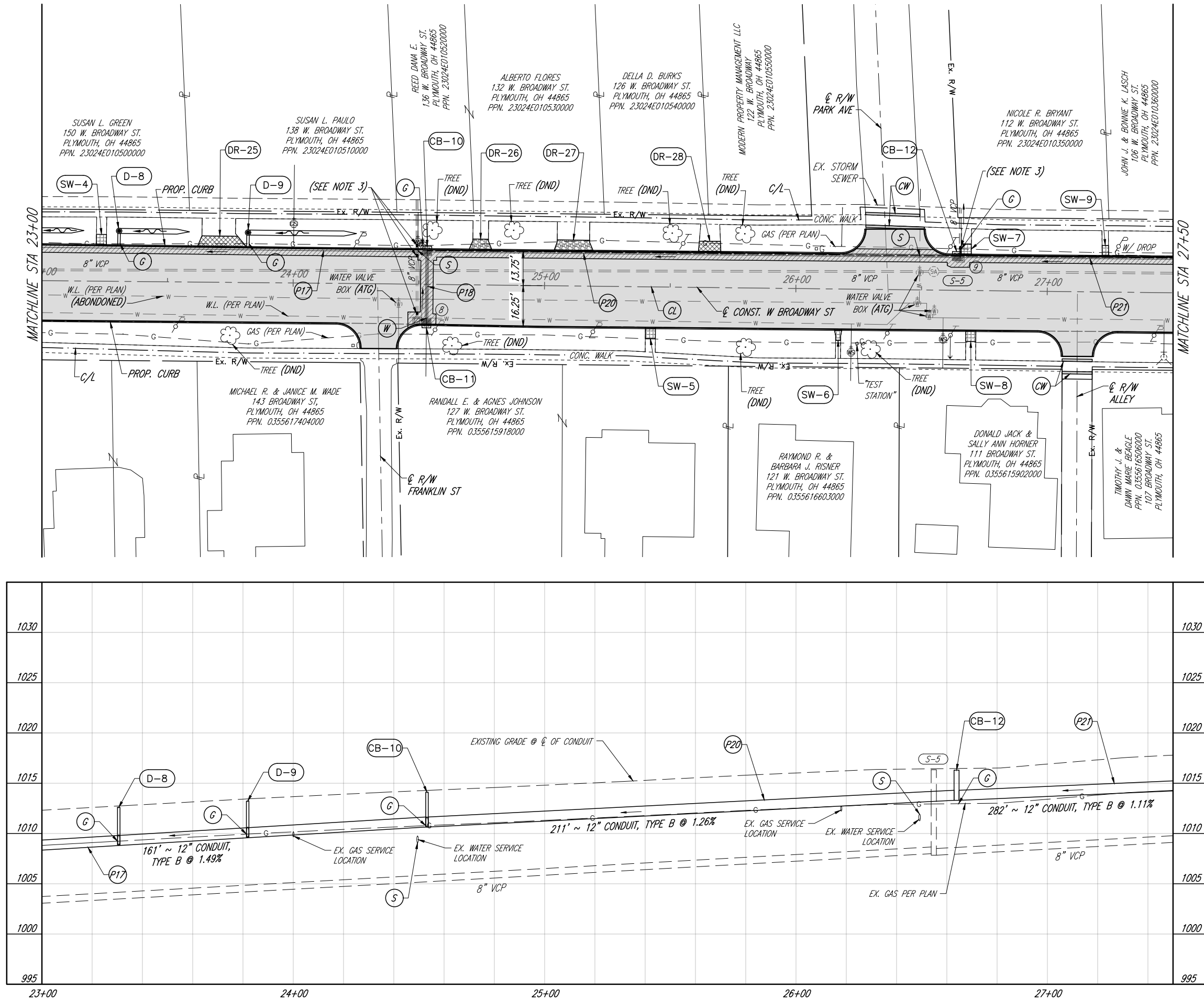
- SEE SHEET 11 FOR ITEM LEGEND.
- SEE SHEET 16 FOR STORM SEWER LATERAL CONNECTION PROFILES.
- SEE SHEET 16 FOR STORM SEWER PROFILES ON THE SOUTH EDGE OF WEST BROADWAY STREET.
- CONTRACTOR TO FIELD LOCATE THE INTERSECTION AND INVERT ELEVATIONS OF THE EXISTING STORM SEWER. DRAINAGE STRUCTURES P8, P9, AND MH-1 MAY NEED TO BE ADJUSTED IN ORDER TO TIE INTO THE EXISTING STORM SEWER.
- EXISTING PIPE TO BE ABANDONED IN PLACE AND PLUGGED IN ACCORDANCE WITH DETAIL Z ON SHEET Z.
- MAKE CONNECTION TO EXISTING CONDUIT WITH CONCRETE COLLAR AS PER ODOT SCD DM-1.1
- CONTRACTOR TO MATCH UNKNOWN PIPE SIZE OF EXISTING CONDUIT.

- UTILITY NOTES:
- ALL UTILITIES LABELED "PER PLAN" WERE NOT MARKED IN THE FIELD AND WILL NEED TO BE VERIFIED BY THE OWNER.
 - EXISTING WATER LINES ARE SHOWN AT A 54" DEPTH. EXISTING GAS LINES ARE SHOWN AT A 42" DEPTH. THE CONTRACTOR SHALL CONFIRM THE LOCATION AND DEPTH OF ALL UTILITIES IN THE FIELD.
 - ALL UTILITY CROSSINGS CALLED OUT CROSS AT THE PROPOSED STORM SEWER.
 - SEE SHEET 8 FOR WATER MAIN AND SERVICE LINE LOWERING DETAILS

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- 8 STA 24+49.45, 14.72' RT
EX. CATCH BASIN
(TO BE REMOVED)
1013.73 T/GRATE
1012.08 INV. 8" VCP N
- 9 STA 26+65.27, 12.87' LT
EX. CATCH BASIN
(TO BE REMOVED)
1016.17 T/GRATE
1013.42 INV. 8" VCP N
- 17 161' ~ 12" CONDUIT,
TYPE B, @ 1.49% SLOPE
- 10 STA 24+53.21, 13.75' LT
PR CB, ODOT No. 3A
1014.11 T/GRATE
1010.64 INV 12" (S)
1010.64 INV 12" (E)
1010.64 INV 12" (W)
- 18 30' ~ 12" CONDUIT,
TYPE B, @ 0.97% SLOPE
(SEE NOTE 2)
- 11 STA 24+53.21, 15.90' RT
PR CB, ODOT No. 3A
1013.93 T/GRATE
1010.93 INV 12" (N)
- 19 NOT USED
- 20 211' ~ 12" CONDUIT,
TYPE B, @ 1.26% SLOPE
- 12 STA 26+63.88, 14.00' LT
PR CB, ODOT No. 3A
1016.29 T/GRATE
1013.29 INV 12" (W)
1013.29 INV 12" (E)
- 21 282' ~ 12" CONDUIT,
TYPE B, @ 1.11% SLOPE
- S-5 STA 26+54.81, 7.36' LT
EX. SANITARY MANHOLE
(ADJUST TO GRADE)
1016.34 T/CASTING
1007.84 INV. 8" VCP E
1007.99 INV. 8" VCP W
- D-8 STA 23+30.58, 20.07' LT
PR 12" YARD DRAIN
1012.60 T/GRATE
1008.86 12" (S)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE
- D-9 STA 23+81.87, 19.89' LT
PR 12" YARD DRAIN
1013.22 T/GRATE
1009.63 12" (S)
5' ~ 12" CONDUIT,
TYPE C, @ 1.00% SLOPE

- NOTES:
- SEE SHEET 11 FOR ITEM LEGEND.
 - SEE SHEET 16 FOR STORM SEWER LATERAL CONNECTION PROFILES.
 - EXISTING PIPE TO BE ABANDONED IN PLACE AND PLUGGED IN ACCORDANCE WITH DETAIL Z ON SHEET Z.
- UTILITY NOTES:
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 - ALL UTILITY CROSSINGS CALLED OUT CROSS AT THE PROPOSED STORM SEWER.



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- (10) STA 29+39.86, 14.27' RT
EX. CATCH BASIN
(TO BE REMOVED)
1019.74 T/GRATE
1017.14 INV. 10" VCP W
- (S-6) STA 30+42.50, 9.23' LT
EX. SANITARY MANHOLE
(ADJUST TO GRADE)
1022.21 T/CASTING
1013.01 INV. 8" VCP W

- (P21) 282' ~ 12" CONDUIT,
TYPE B, @ 1.11% SLOPE

- (CB-13) STA 29+45.74, 14.69 LT
PR CB, ODOT No. 3A
1020.19 T/GRATE
1016.42 INV 12" (S)
1016.42 INV 12" (W)

- (P22) 31' ~ 12" CONDUIT,
TYPE B, @ 0.97% SLOPE
(SEE NOTE 2)

- (CB-14) STA 29+45.91, 15.86' RT
PR CB, ODOT No. 3A
1019.72 T/GRATE
1016.72 INV 12" (N)

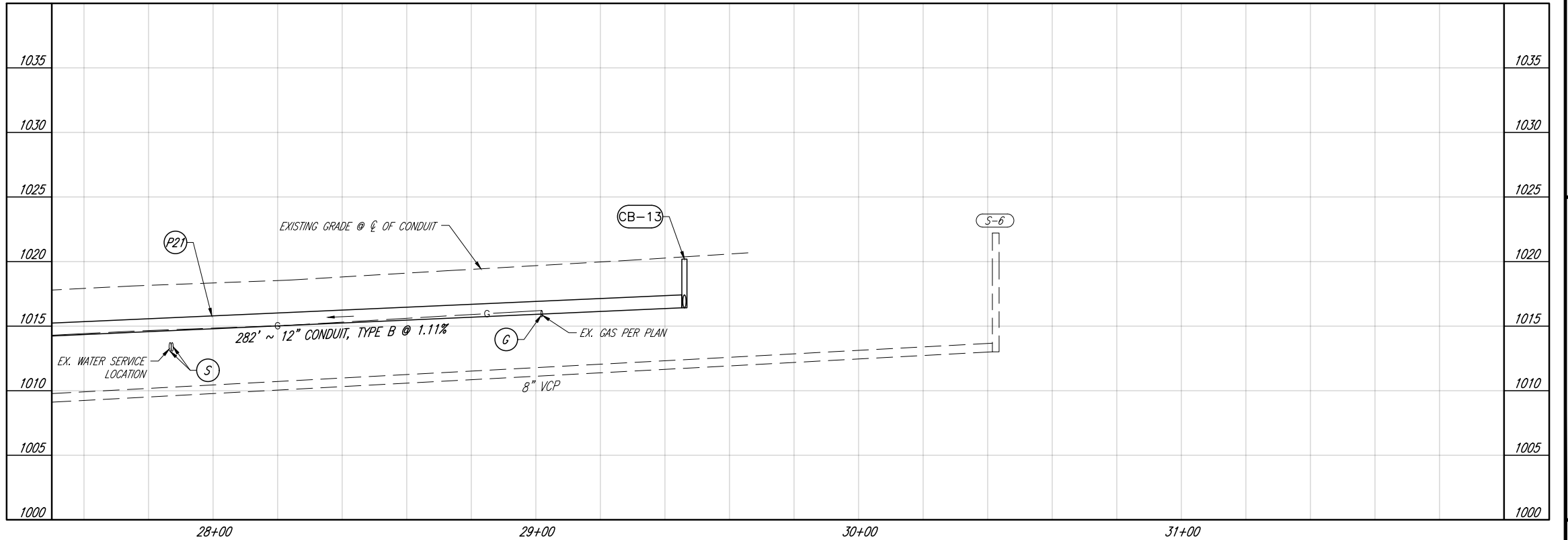
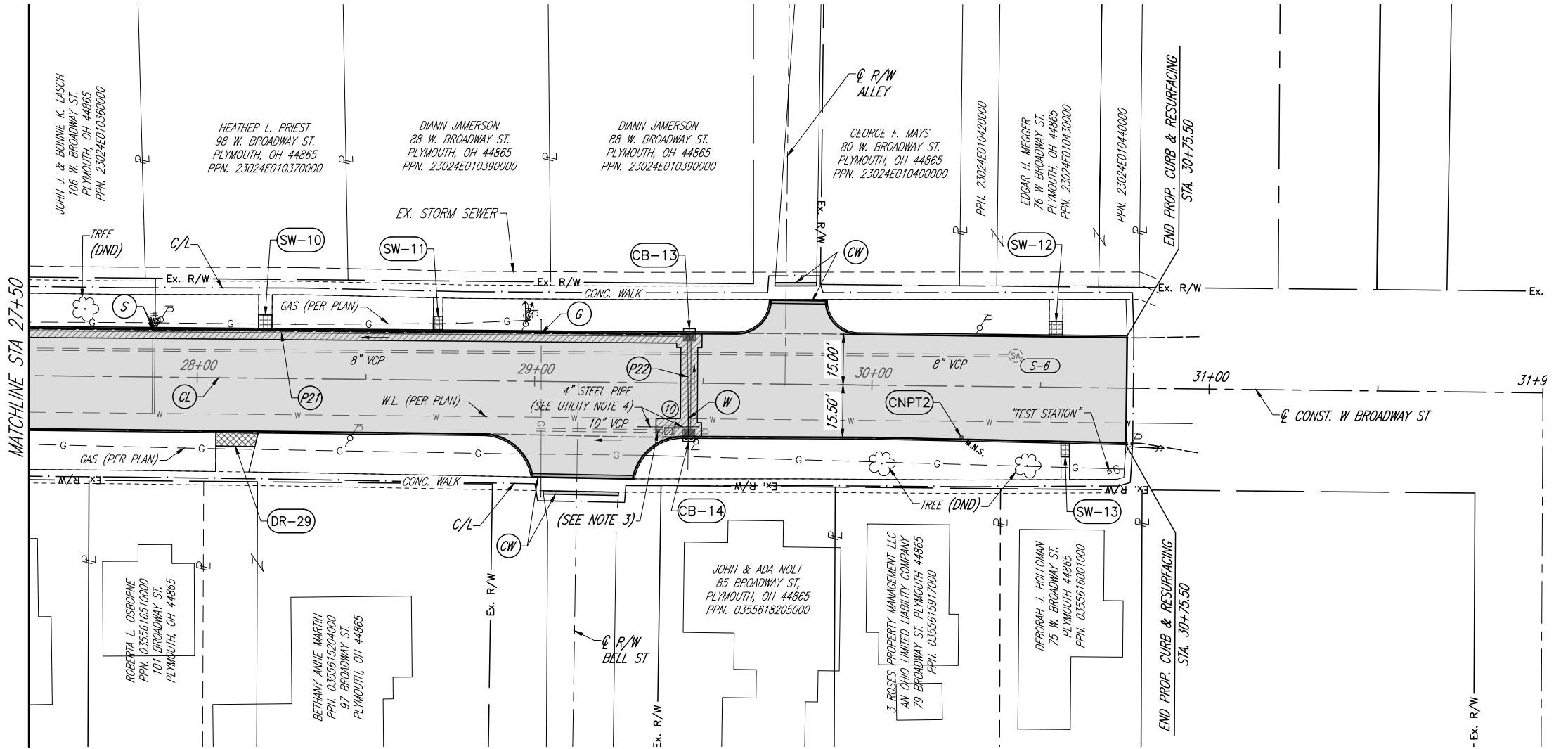
CONTROL POINT #2 (CNPT2)
STA 30+26.76, 15.17' RT
@ CONST. W BROADWAY ST
~ MAG NAIL SET
ELEV: 1021.73'

NOTES:

- SEE SHEET 11 FOR ITEM LEGEND.
- SEE SHEET 16 FOR STORM SEWER LATERAL CONNECTION PROFILES.
- EXISTING PIPE TO BE ABANDONED IN PLACE AND PLUGGED IN ACCORDANCE WITH DETAIL Z ON SHEET Z.

UTILITY NOTES:

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- ALL UTILITY CROSSINGS CALLED OUT CROSS AT THE PROPOSED STORM SEWER.
- 4" STEEL PIPE TO BE ABANDONED IN PLACE BY OTHERS. THE SECTION NEAR EXISTING CATCH BASIN 10 AND PROPOSED CATCH BASIN CB-14 NEEDS TO BE REMOVED.



0 10 20 40
HORIZONTAL
SCALE IN FEET

DESIGNED
TCW
REVIEWED
JDB

K.E. McCARTNEY & ASSOCIATES
ENGINEERS • PLANNERS • SURVEYORS

PLAN AND PROFILE - W. BROADWAY ST.
STA 27+50 TO END

VILLAGE OF PLYMOUTH
DRAINAGE IMPROVEMENTS

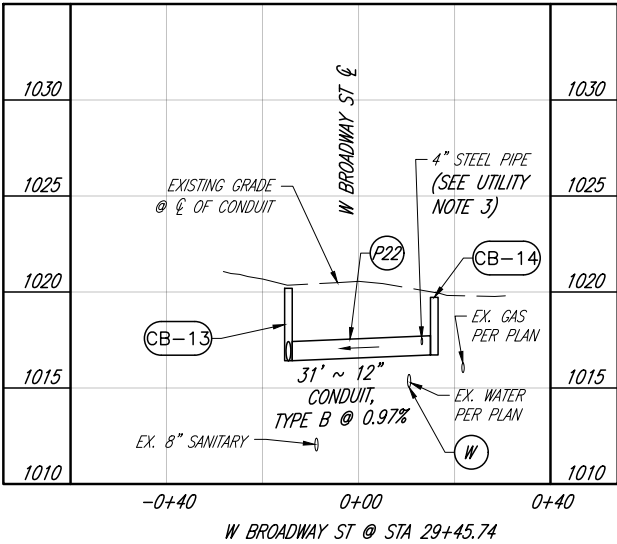
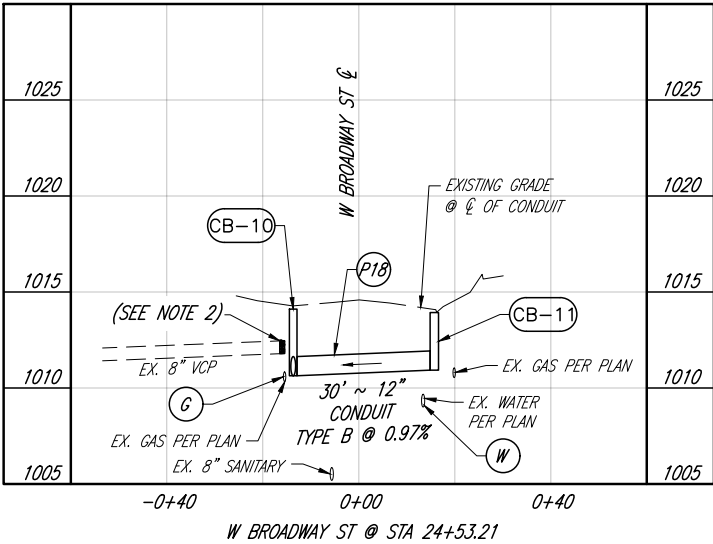
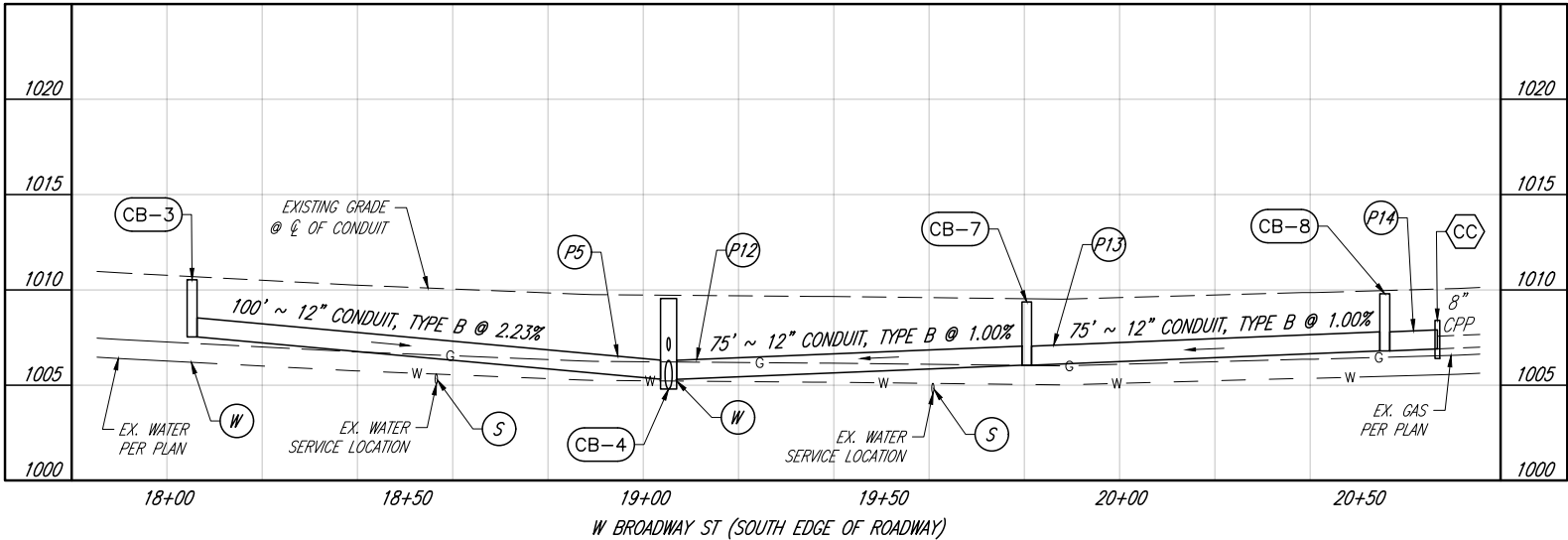
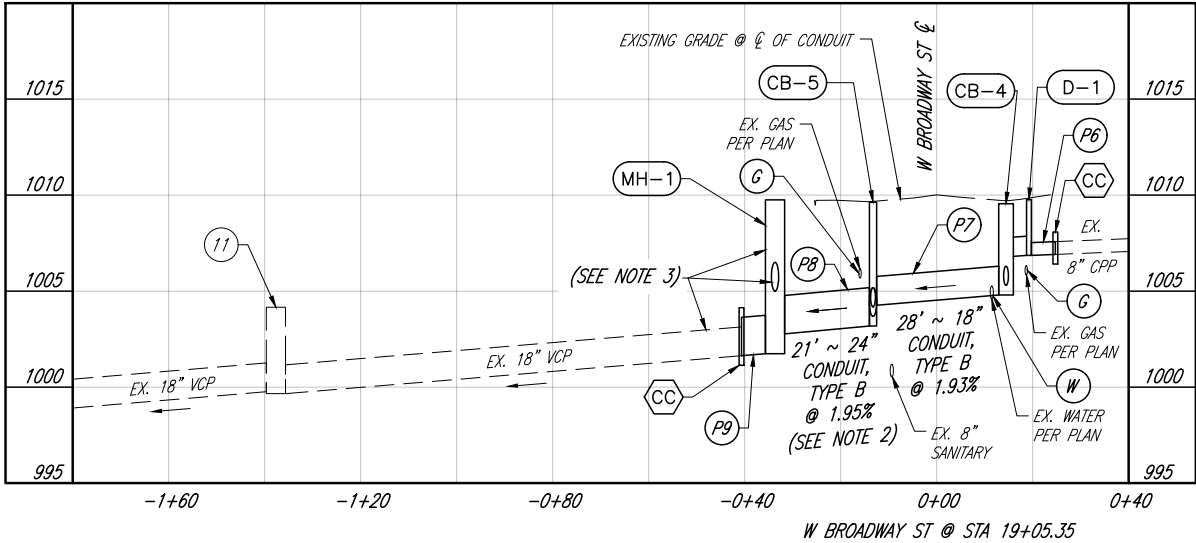
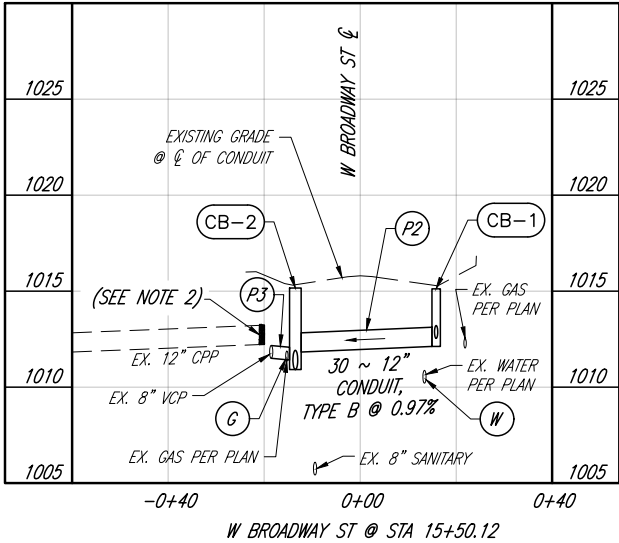
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16

NOTES:

1. SEE SHEET 11 FOR ITEM LEGEND.
2. EXISTING PIPE TO BE ABANDONED IN PLACE AND PLUGGED IN ACCORDANCE WITH DETAIL Z ON SHEET Z.
3. CONTRACTOR TO FIELD LOCATE THE INTERSECTION AND INVERT ELEVATIONS OF THE EXISTING STORM SEWER, DRAINAGE STRUCTURES P8, P9, AND MH-1 MAY NEED TO BE ADJUSTED IN ORDER TO TIE INTO THE EXISTING STORM SEWER.

UTILITY NOTES:

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2. EXISTING WATER LINES ARE SHOWN AT A 54" DEPTH. EXISTING GAS LINES ARE SHOWN AT A 42" DEPTH. THE CONTRACTOR SHALL CONFIRM THE LOCATION AND DEPTH OF ALL UTILITIES IN THE FIELD.
3. 4" STEEL PIPE TO BE ABANDONED IN PLACE BY OTHERS. THE SECTION NEAR PROPOSED CATCH BASIN CB-14 NEEDS TO BE REMOVED.



DESIGNED
TCW

REVIEWED
JDB

KEM
K.E. MCCARTNEY & ASSOCIATES
ENGINEERS-PLANNERS-ARCHITECTS

STORM SEWER PROFILES

VILLAGE OF PLYMOUTH
DRAINAGE IMPROVEMENTS